



LAKWOOD CITY COUNCIL AGENDA

Monday, June 20, 2016

7:00 P.M.

City of Lakewood

City Council Chambers

6000 Main Street SW

Lakewood, WA 98499

Page No.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

PROCLAMATIONS AND PRESENTATIONS

1. Business showcase. – Bite Me! – *Ms. Deborah Tuggle, Owner*
2. Recognizing the City of Lakewood 20th Anniversary letter-writing competition winners and student art murals.

PUBLIC COMMENTS

C O N S E N T A G E N D A

- (4) A. Approval of the minutes of the City Council meeting of June 6, 2016.
- (11) B. Approval of the minutes of the Joint City Council and Clover Park School District Board meeting of June 11, 2016.
- (25) C. Approval of the minutes of the City Council Study Session of June 13, 2016.

The Council Chambers is accessible to persons with disabilities. Equipment is available for the hearing impaired. Persons requesting special accommodations or language interpreters should contact the City Clerk's Office, 589-2489, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

City Hall will be closed 15 minutes after adjournment of the meeting.

(30) D. Resolution No. 2016-11

Accepting a grant from the Washington State Department of Commerce, in the amount of \$50,000, for a JBLM North Clear Zone Land/Property valuation and relocation analysis.

(34) E. Motion No. 2016-26

Awarding a bid to Granite Construction Company, in the amount of \$250,000, for the 2016 chip sealing program.

(37) F. Motion No. 2016-27

Authorizing the execution of Amendment No. 1 to the agreement with Bruce Dees and Associates, in the amount of \$53,113, for a total of \$97,578, relative to the Springbrook Park improvement project.

(43) G. Items Filed in the Office of the City Clerk:

1. Landmarks and Heritage Advisory Board meeting minutes of April 28, 2016.

R E G U L A R A G E N D A**ORDINANCE**(46) Ordinance No. 643

Granting a franchise to Astound Broadband, LLC, dba Wave, to install, operate and maintain a fiber optic-based telecommunications system within the public rights-of-way in the city of Lakewood. – *Assistant to the City Manager*

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RESOLUTION(84) Resolution No. 2016-12

Adopting the 2017-2022 Six Year Transportation Improvement Program. –
Public Works Director

UNFINISHED BUSINESS**NEW BUSINESS****REPORTS BY THE CITY MANAGER****CITY COUNCIL COMMENTS****ADJOURNMENT**

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LAKWOOD CITY COUNCIL MINUTES

Monday, June 6, 2016
City of Lakewood
City Council Chambers
6000 Main Street SW
Lakewood, WA 98499

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:01 p.m.

ROLL CALL

Councilmembers Present: 6 – Mayor Don Anderson; Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Marie Barth and Paul Bocchi.

Councilmember Excused: 1 – Councilmember John Simpson (arrived at 7:25 p.m.).

PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was led by Mayor Anderson.

PROCLAMATIONS AND PRESENTATIONS

Youth Council recognition and report.

Youth Councilmembers presented their annual report for 2015-2016 and described the various activities they participated in.

Mayor Anderson recognized the 2015-2016 outgoing Youth Councilmembers and presented them with certificates of appreciation.

Ms. DeAnne Bennett from Lakewood First Lions Club presented Marcos Vieyra with a Scholarship Award.

Proclamation recognizing Ray and Dottie Evans.

Councilmember Simpson arrived at 7:25 p.m.

MAYOR ANDERSON PRESENTED A PROCLAMATION RECOGNIZING RAY AND DOTTIE EVANS TO MR. ROSENDO GUERRERO, TROUT UNLIMITED, ON BEHALF OF RAY AND DOTTIE EVANS.

Proclamation recognizing Lakes High School students as the #40 Best High School in the state by U.S. News & World Report. – *Ms. Hollee Foster, ASB President, Lakes High School*

MAYOR ANDERSON PRESENTED A PROCLAMATION RECOGNIZING LAKES HIGH SCHOOL STUDENTS AS THE #40 BEST HIGH SCHOOL IN THE STATE BY U.S. NEWS & WORLD REPORT TO MS. HOLLEE FOSTER, ASB PRESIDENT, LAKES HIGH SCHOOL.

Clover Park School District Board Report.

Clover Park School District (CPSD) Board Director Vlaming commented on the upcoming high school graduation ceremonies and the CPSD water survey results. He then spoke about the summer school schedules and provided an update on the Building Facilities Committee activities and school improvements.

PUBLIC COMMENTS

Speaking before the Council was:

Dennis Haugen, Lakewood resident, showed a video on emergency management.

Rebecca Lee, Lakewood Village Apartment resident, expressed concern about the need for street lights near Four Heroes Elementary and Harrison Preparatory School and concerns when walking along Lakeview Avenue.

C O N S E N T A G E N D A

- A. Approval of the minutes of the City Council meeting of May 16, 2016.
- B. Approval of the minutes of the City Council Study Session of May 23, 2016.
- C. Approval of payroll checks, in the amount of \$2,309,767.63, for the period April 16, 2016 through May 15, 2016.
- D. Approval of claims vouchers, in the amount of \$2,128,077.40, for the period April 16, 2016 through May 13, 2016.
- E. Motion No. 2016-22

Appointing Johnny Williams to serve on the Public Safety Advisory Committee through August 6, 2019.

F. Motion No. 2016-23

Appointing DeeAnn Harris to serve on the Community Services Advisory Board through December 15, 2020.

G. Motion No. 2016-24

Appointing Robert Fox to serve on the Lakewood Arts Commission through October 15, 2017.

H. Motion No. 2016-25

Authorizing the execution of Amendment No. 1 to the agreement with Bruce Dees and Associates, in the amount of \$24,000, for a total of \$69,700, relative to the Waughop Lake Trail project.

I. Items Filed in the Office of the City Clerk:

1. Planning Commission meeting minutes of April 20, 2016.
2. Public Safety Advisory Committee meeting minutes of April 6, 2016.
3. Parks and Recreation Advisory Board meeting minutes of April 19, 2016.

COUNCILMEMBER MOSS MOVED TO ADOPT THE CONSENT AGENDA AS PRESENTED. SECONDED BY COUNCILMEMBER SIMPSON. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

R E G U L A R A G E N D A

PUBLIC HEARINGS AND APPEALS

This is the date set for a public hearing on the 2017-2022 Six Year Transportation Improvement Program.

Speaking before the Council was:

Dennis Haugen, Lakewood resident, expressed concern about economic planning and that transportation planning needs to relate to economic planning.

Michael Brandstetter, Lakewood resident, spoke about a citizen's comment that was provided before the Council tonight about the need for a crosswalk at Lakewood Drive between 100th Street and Steilacoom Boulevard and supported it being in the Six Year Transportation Improvement Program.

There being no further testimony, the hearing was declared closed.

ORDINANCES

Ordinance No. 641 creating Sections 15.05.010, 15.05.020, 15.05.030, 15.04.040, 15.05.060, 15.05.070, 15.05.090, and repealing Chapters 15A.08, 15A.10, 15A.12, 15A.14, 15A.16, 15A.20, 15A.22, 15A.24, 15A.26, 15A.28, 15A.30, 15A.32 and 15A.34 of the Lakewood Municipal Code relative to adopting the 2015 edition of the International Code Council.

COUNCILMEMBER BRANDSTETTER MOVED TO ADOPT ORDINANCE NO.641. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

Ordinance No. 642 amending Section 12A.15.060 of the Lakewood Municipal Code relative to mandatory sewer connections.

DEPUTY MAYOR WHALEN MOVED TO ADOPT ORDINANCE NO.642. SECONDED BY COUNCILMEMBER SIMPSON. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

UNFINISHED BUSINESS

None.

NEW BUSINESS

None.

REPORTS BY THE CITY MANAGER

City Manager Caulfield called on Assistant City Manager for Development Services Bugher to present the next two agenda items.

Lakewood Municipal Code Title 18A Land Use and Development update.

Assistant City Manager for Development Services Bugher provided an overview of the proposed changes to the organization and structure of Title 18A of the Land Use and Development code.

Discussion ensued on how housing rental units will be handled for those who cannot afford maintenance costs; and are the centers of local importance identified in Title 18A (identified in Comprehensive Plan).

Rental housing safety program update.

Assistant City Manager for Development Services Bugher reviewed the draft budget for implementing a proposed rental housing safety program.

Discussion ensued on the effect of placing liens on rental housing properties; how will a private housing inspector list be generated and what is the expected split on the number of City conducted inspections versus private inspections; how will the process work when private inspectors find safety issues; how will the random split of single family and multi-family inspections be generated; what information is available to the public about the proposed rental housing inspection program for citizens to comment on at the public hearing; and posting the rental housing registration and safety inspection program general information report that is in the Council agenda packet tonight on the website.

City Manager Caulfield noted that thank you letters are being prepared for those who participated in the 20th Anniversary summer celebration. He thanked the 20th Anniversary Committee members who organized the event.

He reported on a summit meeting he attended to discuss the new Lakewood Library. He then indicated that he was approached by the Pierce County Deputy Executive, who asked if the City would support a joint resolution with Pierce County and the cities of Lakewood, University Place, Tacoma and Steilacoom to work together in supporting another US Open at the Chambers Bay Golf Course. He also reported that several meetings were held with Mr. Fed Roberson who is interested in donating property located where Pacific Highway south is crossed by Clover Creek in 2017 for tax purposes. He noted that Mr. Roberson is asking that the City provide a curb cut to Pacific Highway South to the other property he owns that is adjacent to the one he wishes to donate.

He then reviewed the six transportation improvement project applications the Public Works Department has submitted for Pierce County Regional Council's county-wide grant funding.

He reported that work on the final design of the gathering place at Fort Steilacoom Park is moving forward.

He then reported that the Governor has appointed a Select Committee on Quality Improvement in State Hospitals that will be meeting on June 30, 2016 which the City will be hosting in the Council Chambers from 1:00 p.m. to 4:00 p.m. The City will have an opportunity to present the Western State Hospital Community Policing Program and Capital Facilities Planning at Western State Hospital before the Committee.

City Manager Caulfield noted that in following-up with Sound Transit 3, a number of items that had been brought forward to be considered and addressed that were not, were noted by Sound Transit 3 and they indicated they will be responded to.

He reported that the Pierce County Council is moving forward with a Resolution to transfer Tacoma Mall Boulevard right-of-way from 84th to 80th Street to the City of Lakewood, per a 1999 agreement between Tacoma and Lakewood. This will help to avoid confusion between various law enforcement agencies on who is

responsible for traffic violations and responding to accidents. He indicated that this will be on the Council's agenda for June 13, 2016.

He announced that City staff will be participating in Cascadia Rising emergency exercise on June 8, 2016 operating out of the Emergency Operations Center. He also noted that the County is moving forward with a health behavioral systems study.

He then reported on the following calendar items:

- June 7, 10:00 AM to 3:00 PM, Farmer's Market with Opening Ceremony at 11:45 AM, Lakewood City Hall
- June 11, 9:00 AM to 11:00 AM, Joint CPSD Board/Lakewood City Council Meeting, CPSD Student Services Center, Room 4, 10903 Gravelly Lake Dr. SW
- June 15, 8:00 AM to 10:30 AM, Developer's Forum, McGavick Center

CITY COUNCIL COMMENTS

Mayor Anderson asked if the Council was interested in joining the County, University Place, Tacoma and Steilacoom in supporting a joint resolution for another the U.S. Open at Chambers Bay Golf Course. It was the consensus of the Council to support such a resolution and to place it on the Council's June 20, 2016 agenda for consideration.

Councilmember Moss commented on the 2-2 event she attended. She also spoke about the ground breaking Sound Transit ceremony she attended on 25th Street. She also spoke about the Relay for Life event she participated in.

Councilmember Bocchi spoke about the 20th Anniversary summer celebration and thanked the Committee for organizing the event. He reported that he will be attending the Lake City Neighborhood Association meeting.

Councilmember Brandstetter spoke about his trip to Alaska. He commented on the Mt. View Funeral Home Memorial event he attended. He also commented on the Relay for Life event he participated in. He commented on the 20th anniversary celebration, the letter-writing contest and the focus on youth.

Councilmember Barth commented on the sister city event at Pierce College and the sister city relationship with Korea and Taste of Korea. She then commented on the 20th Anniversary event.

Councilmember Simpson commented on the Relay for Life event and the Memorial Day event at Mt. View Funeral Home. He indicated that he will attend the Cascadia Rising event.

Deputy Mayor Whalen commented on the 20th Anniversary event. He also commented on the 2-2 social event he attended. He asked something could be done about a pedestrian lighted crosswalk that was suggested by the concerned citizen who provided public comment. He then noted that Tacoma Chamber of Commerce and the Economic Development Board had filed a lawsuit about the save our water initiative.

Mayor Anderson spoke about Farmers Market opening on June 7, 2016. He commented on the 2-2 Stryker event he attended at Lakewold Gardens. He then spoke about the Senior Center open house he attended. He commented about the Relay for Life event. He spoke about the 20th Anniversary summer event and the Jack Nicklaus 9 golf event he attended.

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Executive Session

Mayor Anderson announced that Council will recess into Executive Session for approximately 15 minutes to discuss purchase and sale of real property.

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Council recessed into Executive Session at 10:05 p.m. and reconvened at 10:20 p.m.

ADJOURNMENT

There being no further business, the meeting adjourned at 10:20 p.m.

DON ANDERSON, MAYOR

ATTEST:

ALICE M. BUSH, MMC
CITY CLERK



**LAKWOOD CITY COUNCIL
AND CLOVER PARK
SCHOOL DISTRICT BOARD
JOINT MEETING MINUTES**

Saturday, June 11, 2016
Clover Park School District Student Services Center
Room 4
10903 Gravelly Lake Drive SW
Lakewood, WA 98499

CALL TO ORDER

Mayor Anderson and Clover Park School District President Schafer called the meeting to order at 9:03 a.m. and introduced Mr. Dave Coleman, facilitator for the joint meeting.

ATTENDANCE

Councilmembers Present: 7 – Mayor Don Anderson; Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, John Simpson, Marie Barth and Paul Bocchi.

City of Lakewood staff present: 4- City Manager John Caulfield, Assistant City Manager for Development Services Dave Bugher, City Attorney Heidi Wachter, and City Clerk Alice M. Bush.

Clover Park School District Board Members Present: 5 -President Marty Schafer, Vice President Carole Jacobs, Directors Joe Vlaming, Becki Kelcy, and Paul Wagemann.

Clover Park School District staff Present: 2- Superintendent Debbie LeBeau, and Executive Assistant Carmen Lewis.

INTRODUCTIONS

Each participant introduced themselves and described their expectation for today's meeting.

Facilitator Coleman then described that the purpose of the meeting is to have a productive conversation around the necessity for the School District and City government to collaborate, more specifically, working together to decide the future of the Woodbrook Middle School.

Mr. Coleman envisioned the upcoming CPSD Facilities Advisory Committee (FAC) and the City's land use and economic development policies playing major roles in this process.

Thereafter, Mr. Coleman reviewed the meeting protocols, "Rules of the Road" for the group to follow in today's meeting and the importance of collaboration.

BACKGROUND

Mr. Coleman reviewed assumptions of where the School District and the City were in alignment:

- Lakewood is a place where families and children thrive;
- CPSD provides excellent public education;
- Lakewood has affordable, quality housing;
- City government takes actions that positively influence education;
- Collaboration exists between the School District and the City where both parties work together with each other's best interests in mind;
- Respecting differences in priorities, and in risk-taking;
- Know with certainty that at some future date the current Woodbrook Middle School facility will be closed and vacated, but that the specificity of how and when that will take place is uncertain at this point in time; further, each entity (School District and City) has its own priorities, perspectives and desired timetables on how and when those uncertainties can become more certain.

Mr. Coleman then reviewed the Woodbrook Middle School's history. He explained how the school is currently intertwined with the City's economic development policies.

He explained that in the early days of cityhood, circa 2000, Woodbrook Middle School was zoned as Public Institutional. In 2004, the City began the process to install sewers in both Woodbrook and Tillicum. At that time, it was recognized that the middle school was in decline. There were uncertainties in the planning process that centered on military land use issues, the economy, and the proposed Cross-Base Highway.

In 2009, two important decisions are made. CPSD's FAC recommended that Woodbrook Middle School be relocated to Ft. Lewis. At the same time, the City began discussing the planning process for the Woodbrook area as a future business park. (*Woodbrook Business Park Development Report*)

Two years later, improvements are planned for Murray Road and the Department of Defense made it clear that they will not fund the construction of an on-base middle school.

In 2012, a strategic plan envisioned a Woodbrook Business Park and zoning was changed to industrial business. The City believes that CPSD will eventually close Woodbrook Middle School and surplus the property. Meanwhile, CPSD has not yet decided the fate of Woodbrook Middle School, and is busy finishing Lakes High School and building eight new schools, both on the base and in the city. Because of this, CPSD skips one FAC planning cycle.

With open space land limitations, CPSD locates Four Heroes and Harrison Preparatory on land adjacent to Clover Park Technical College.

In 2015, City grants a conditional use permit for a new 400,000 square foot warehouse in Woodbrook, and the State adopts a six year I-5 improvement plan.

In the fall of 2015, letters are exchanged between the Mayor and School Board President. City would like to see Woodbrook Middle School closed in the fall of 2016 in order to take advantage of a favorable business climate surrounding the development of a Woodbrook Business Park. CPSD provides City with various documents, but is not ready to make a decision about Woodbrook Middle School's eventual closure and the disposition of the property. It will utilize a 2016-17 FAC study with public recommendations related to school district facility needs. Woodbrook Middle School will likely be high on the list for relocation of students and replacement of classroom space, with specific decisions to follow a formal data driven study involving community-based recommendations. While the City would be represented in the FAC process, the City would at least like a date when it anticipates that the middle school would be vacated and CPSD would be ready to sell the property, a process that will follow certain specified legal guidelines.

Mayor Anderson spoke about redevelopment of the Woodbrook/industrial park development which will result in the following benefits:

- The youth and community
- Job creation and economic development
- A WSDOT maintenance facility relocating to the area;
- The City's investment of infrastructure for industrial development, thereby enhancing the overall value of the property, and street improvements
- WSDOT's investment in I-5 related improvements.
- Improved community image;
- New retail development;
- Reducing parental unemployment and provide students employment and training opportunities;
- New and improved uses assessed valuations; and
- Increased sales and tax revenues for State, City, Pierce Transit, Pierce County, Library and Sound Transit.

He indicated that WSDOT's facilities analysis (Pacific Highway site) needs to pursue a parallel track to explore the Woodbrook site as a possibility, and it needs specificity for a date it will be vacated. He again indicated that there was urgency, since market conditions tend to be cyclical.

Councilmember Barth expressed the importance of timing for developers when it comes to real estate development and the benefits of economic development for students and the community as a whole.

Board Member Wagemann asked if there was flexibility on the date or is there a drop dead date.

Mayor Anderson noted that if it is six months then a developer would likely move forward. If it is 1 ½ years out, then the further the date is moved out, it becomes more and more uncertain.

Deputy Mayor Whalen asked if state legislative approval is required for the relocation of the WSDOT facility.

Mayor Anderson indicated that there is state legislative support, and that the proceeds from the sale of the existing property would be allocated for the relocation of this project.

There was general discussion on teachers and the existing school – how these things promote value for the overall community.

Further discussion ensued with students being cyclical, where would the students relocate once the school is closed.

President Schafer spoke about increasing the ability for the School District and the City to communicate as a team and the importance of collaboration. He noted that CPSD is the only school district in the state that has built as many schools as it has in a short period of time and received a number of distinguished recognition, achievement awards, outstanding achievements and established the first International Baccalaureate school program.

Superintendent LeBeau spoke about some of the constraints on the school district. She mentioned state and federal mandates, regulatory processes that must be followed, issues over limited bonding capacity, insufficient land to plan for new schools, and should vacant CPSD property be leased or sold. She explained that accelerating the FAC process provides the opportunity for timely options in the 2017-2018 timeframe.

When asked using a hypothetical timeframe, and if the FAC were to recommend a closure of Woodbrook Middle School in January 2017, what would the School District need to do?

The answer – first, the School Board would need to agree to close the school. After that, decisions would have to be made as to where the current students would be housed. The next step would be changing district boundaries and parent notification. Other factors to consider are how quickly can another school be constructed and what options are there; what is the District's bonding capacity; is this process in the best interest for the District and for how many years; and where would the school district build a new middle school.

Board Member Jacobs indicated that citizens will be asked to consider another bond to build another school in areas that don't have a high voting base to continue a replacement package.

Deputy Mayor Whalen indicated that there is a limited supply of funding and the collective goal is to increase the economic pie so that the residential homeowners don't continue to have a disproportionate share of the tax burden. If we increase the industrial/ commercial opportunities, that will help increase the opportunity to get more bonds passed to spread the tax base.

Councilmember Simpson noted that the economic impact of creating approximately 1,600 jobs to the City would amount to \$57,000 an hour and the longer that we delay, that is what we are walking away from.

Councilmember Brandstetter asked for clarification about the District's Facilities Master Plan and what is the plan for Woodbrook Middle School.

It was explained that the District's facilities master plan is a four year plan. The last plan was done in 2009, indicating that a new middle school was to be constructed on JBLM. However, in 2010, JBLM changed its position and did not support a new middle school on post. MS. LeBeau stated there was a significant amount of data for the FAC to review.

Mayor Anderson noted that there is some indication that JBLM is open to offer a site for a middle school on JBLM. Superintendent LeBeau noted that the FAC would need to consider such a proposal.

Recessed at 10:40 a.m. and reconvened at 11:00 a.m.

PROBLEM SOLVING

Facilitator Coleman then asked members to identify problem solving issues:

City of Lakewood

- Timing – be specific – and the sooner the better!
- Take advantage of opportunity
- Need for flexibility and adaptation
- Need to speak with one voice
- Need collective communication
- Common appreciation for economic development
- Balance between process and outcome
- What does economic development mean – focus on creating jobs, reducing parental unemployment, creating summer jobs, learning trades with the end result is raising the bar for everyone
- City provides the zoning, infrastructure for economic development

Clover Park School District

- Timing – be specific – this is our facility
- Factors – data, people, think long-range
- Speaking with one voice
- Decision for Woodbrook Middle School, after January 2017

Discussion ensued on the mechanism for providing ideas and input to the FAC. CPSD indicated that input would be provided at the front end of the FAC's process.

The CPSD cannot make a decision about Woodbrook Middle School in accordance with CPSD policy until after January 2017. FAC needs detailed demographic data. It was pointed out that additional housing is not projected in the Tillicum/Woodbrook area. The North Clear Zone area is not zoned for additional housing and the proposed rental housing safety program could reduce the total number of available rental units.

Board Member Wagemann asked if Woodbrook Middle School could be rebuilt in its current location.

City indicated that no, it cannot be rebuilt in its current location as it is not zoned for public institution.

Board Member Wagemann asked if the property could be traded for another without issuing a bond.

Councilmember Barth asked how this group can fast track actions regarding the middle school property so Lakewood will not miss this economic development opportunity.

Board Director Jacobs expressed concern over student relocation issues within a three month timeframe.

Mayor Anderson indicated that the City is requesting decisions on:

- Certainty that Woodbrook Middle School will close;
- Specificity of date of which it will be vacated; and
- Date the property will be available for redevelopment.

Facilitator Coleman asked if there is a need for a forum for collaboration between the Board and Council.

Mayor Anderson suggested a limited number of representatives from each group on a rotating basis to meet perhaps quarterly including limited staff.

City Manager Caulfield suggested also including representatives from other agencies such as the Fire District and Library.

ADJOURNMENT

There being no further business, the meeting adjourned at 12:09 p.m.

DON ANDERSON, MAYOR

ATTEST:

ALICE M. BUSH, MMC
CITY CLERK

Opening Remarks of Mayor Don Anderson on behalf of the Lakewood City Council, Joint Meeting with Clover Park School District Board June 11, 2016

Thank you for agreeing to meet with us, particularly with respect to your willingness to discuss the future of the Woodbrook Middle School site.

The City desires to pursue policies, practices and programs at the City level which enhance student performance and long term outcomes. Thriving public schools are a cornerstone of a successful community.

We understand that restrictions and limitations are placed on the District by others and that these decisions are difficult. On a personal level, my own experiences make me very aware of those issues. I am a former school board president, my wife is completing her fortieth year as a public school teacher, I tutored for nearly a decade in inner city schools and was a paid coach at a public school.

Our efforts are not aimed directly at what happens inside the walls of the classrooms, that is not our purview, but are on a much broader scale. They transcend traditional assistance such as supporting CISL, Caring for Kids, youth programs and levies, which we continue to support. They are additionally focused on quality and stability in housing, employment and training opportunities, lobbying at the federal level, building an economic base to support bonds and levies and insuring a safe community in which to learn. These factors, external to the classroom, are major influencers of student outcomes.

We see our metrics in these areas moving in a positive direction, but too slowly. Redevelopment of Woodbrook is a key component to enhanced prosperity in Lakewood and as a direct result, enhanced opportunities for and success of our youth.

Woodbrook Industrial Business Park (IBP)

Two major benefits to closure of Woodbrook Middle School (as already identified by CPSD)

1. Allows for significant investment and development of 150 acre Woodbrook IBP for industrial and light manufacturing purposes that will create jobs and opportunity for both Lakewood and our South Sound region
2. Provides a possible option for WSDOT to relocate their maintenance facility which is currently located along Pacific Highway South that in turn will allow for retail development along I-5 frontage totaling 25 acres.

There also is potential integration of 100+ acres to Woodbrook IBP through land swap being considered by JBLM as part of McChord runway encroachment mitigation study.

We believe this development and the changes it enables will be transformational for Lakewood.

Woodbrook IBP

- 150 acres zoned industrial and prime for redevelopment. There is strong interest from the development community to construct this business park given land opportunities around the Port of Tacoma and other areas in the region are limited. We are already being bypassed with investment that should be occurring here is moving to Centralia and Chehalis areas. DOT, Port and private investors are interested.
- In 2009 the District's Facility Advisory Committee recommended that Woodbrook Middle School be relocated to JBLM
- Also in 2009, the City commissioned and released in partnership with a number of agencies the "*Woodbrook Business Park Development Report.*"
- In part, the report discusses the relocation of Woodbrook Middle School as a desirable action and converting the property to a business-related use. This has been discussed for years but without action.
- The report (now 7 plus years old) also outlined the following project benefits:
 - The area will support 1,170 to 3,500 family-wage jobs onsite;
 - Two to three times the number of support jobs located off site or upwards of 10,500;
 - Land and structure value would increase from \$33 million to between \$143 and \$184 million;
 - Based on just 1600 family-wage jobs, the estimated economic impact of the Woodbrook IBP is nearly \$507 million. This includes the direct impact of the new facilities plus the indirect change in employment through vendors and expenditures into the economy. **Using a build out of 3,500 jobs, the total economic impact approaches \$1.0 billion;** and
 - This means great opportunities for our region, particularly Lakewood. These high quality jobs are transformational for local families.
- The City of Lakewood completed a \$6.8 million sewer upgrade to Woodbrook in 2011. It was named a top priority by City leaders upon incorporation and has remained a top priority with the current City Council.
- Phase II, which will complete the necessary upgrades to support the business park, will be finished later this year. This phase costs \$1.6 million.
- The City's long term investment in sewers is specifically for industrial development.

- City leadership has been committed to provide the necessary infrastructure for those neighborhoods to provide the platform for economic revitalization.
- In 2012, the City conducted a Strategic Planning Workshop to help identify potential development activity options in Woodbrook Industrial Park
- Later that year, the City Council amended the Comprehensive Plan and zoning code to designate the Woodbrook Middle School to Industrial Business Park (IBP) that creates tremendous monetary value for the CPSD.
- In 2013, the City's Hearing Examiner approved a conditional use permit application for YKC Investment LLC, with conditions, to construct a 442,382 square foot logistics center on 20 acres that is located directly across the street from the middle school. Employers are reluctant to build or lease on this site when located next to an active school.
- In 2015, the City Council adopted the six-year Transportation Improvement Plan which includes the Murray Road and 150th Street Corridor Capacity projects. These projects will provide capacity for the Woodbrook Industrial Business Park development: widens Murray Road and 150th Street SW; installs bike/pedestrian facilities; and includes structural pavement section improvements. (This is a multi-phased, multi-year project.)
- Later that year, the Washington State Legislature, with strong leadership and support from Lakewood, approves the expenditure of \$495 Million for I-5 improvements from Mounts Road to Thorne Lane Interchange.
- Beginning in 2017, WSDOT proposes to rebuild: the Thorne Lane and Berkeley Street interchanges; add a fourth lane to I-5 in each direction from Thorne Lane to Steilacoom-DuPont Road; & build the Gravelly-Thorne connector local road further facilitating heavy truck access to Woodbrook.

WSDOT Property in Lakewood Economic Benefits of Relocation

Overview

- The City of Lakewood (City) is in discussion with the Washington State Department of Transportation (WSDOT) regarding a property in Lakewood near Interstate 5 (I-5) that is currently owned by WSDOT. This property, along with several adjacent parcels, totaling 25 acres or 1M square feet, may present an opportunity to foster a catalyst-type redevelopment retail project in the city.
- The project site is currently being used for industrial purposes, as a WSDOT maintenance site. This is not the highest and best use for the site. The site's prime I-5 frontage, heavy daily traffic loads (average daily traffic count of 282,000 vehicles per day), and proximity to the Lakewood Sounder Transit Station make it particularly suited for retail development. This is the only urban site in Washington where two freeways intersect without substantial retail development. Redevelopment would only be possible if the WSDOT maintenance site is relocated.
- Between 2014 and 2016, the City has studied and assessed the potential costs associated with relocation.
- The City has also studied fiscal impacts and costs for different development scenarios to the City and other public agencies. The City hopes to bring in an attractive, large anchor retail store to the site, as well as some restaurants or other small retail which would get drivers off the freeway and into Lakewood. There are many possibilities including High-End Retail Development similar to the Tukwila Nordstrom Rack and Kohl's retail site or an Outlet Mall.
- The cost of moving this facility would be significantly lower if WSDOT were to repurpose a portion of the middle school. In addition, they could consolidate offices located throughout Pierce County to this location saving considerable annual office lease payments. They already own some property in the area and feel Woodbrook provides some logistical advantages over their current site.

Multiple Benefits of Relocation

- Benefits for WSDOT and WA State
 - The proposed facility site is 16% larger than the current facility, though can be located on a smaller footprint.

Benefits for the City of Lakewood and Clover Park School District (over and above the proceeds from sale or lease of the land)

- Improve city image.
 - Supports city’s economic development plan.
 - Supports existing retail.
 - Reduces parental unemployment and provides students employment opportunities & training. Studies show that parental unemployment is an even stronger indicator of negative student academic and social outcomes than is income.
 - New and improved uses will increase assessed valuation that will result in lower property tax rate that in turn results in a lower property tax burden for current property owners, particularly single-family property owners = increased opposition to school bonds and levies
- Benefits for Other Jurisdictions
 - Increased retail sales tax and property tax revenues for multiple jurisdictions to include Pierce Transit, Pierce County, Pierce County Library, and the Port of Tacoma, which all stand to receive additional tax revenues

Annual Revenue Estimates

Jurisdiction	Range of Annual Benefit
Washington State	\$4.5M - \$9.2M
City of Lakewood	\$700,000 - \$1.4M
Pierce Transit	\$350,000 - \$800,000
Pierce County	\$175,000 - \$275,000
Pierce County Library	\$25,000
Sound Transit	\$525,000-\$1.2MM

Steps Forward

- Determine WSDOT capacity needs and relocate the maintenance facility. The City has determined a few potential sites for the relocation of the WSDOT maintenance facility. A capacity analysis will also be conducted to determine WSDOT’s site need specific to relocating the maintenance facility.
- Determine the land assembly process to sell as one unit The City would like to begin conversations later this year to determine owners’ willingness to sell, though need certainty on closure of middle school. We also need specificity as to the date.

We are at a critical point where our community needs us to exhibit collective leadership. Government to government coordination is normally easier than public/private endeavors, since we have no hold over private owners. There is a sense of urgency as the market is cyclical. We believe we need to think outside of the box to find both short and long term solutions to housing students from Woodbrook Middle School so that it can be vacated with certainty and by a specified date.



LAKWOOD CITY COUNCIL STUDY SESSION MINUTES

Monday, June 13, 2016
City of Lakewood
City Council Chambers
6000 Main Street SW
Lakewood, WA 98499

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:03 p.m.

ROLL CALL

Councilmembers Present: 6 – Mayor Don Anderson; Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, John Simpson, and Marie Barth.

Councilmember Excused: 1 – Councilmember Paul Bocchi.

ITEMS FOR DISCUSSION:

Alliance for South Sound Behavioral Health Coalition Presentation.

The Alliance for South Sound Behavioral Health Coalition spoke about the need for mental health facilities and asked community partners to assist building a \$42 million hospital at the Allenmore site to be operational in 2018. The Coalition requested that the City pledge \$350,000.

Discussion ensued on how this facility will alleviate homelessness issues in the community; will funding for this project also serve the youth; is the request for funding a one time pledge or is it spread over time; how was Lakewood's amount of \$350,000 determined (primarily population); how much time is the Coalition considering the funding be spread (three years); how will funding those that do not have insurance be handled; Lakewood has been subsidizing other communities with community policing at Western State Hospital, services provided by Greater Lakes Mental Health, the Veterans Hospital and human services funding for mental health issues; and the City is an advocate for mental health and notes that mental health is a State constitution responsibility.

Review of a franchise agreement with Astound Broadband, LLC dba WAVE, to install, operate and maintain a telecommunications system in city rights-of-way.

Assistant to the City Manager Lincoln reviewed a proposed franchise agreement with Astound Broadband to operate a telecommunications system in city rights-of-way. The current project is to run telecommunications fiber to the Lakewood Library.

Discussion ensued if the agreement also includes fiber to the Tillicum Library; how many miles of rights-of-way is the initial project; is the project underground (project is above ground); and is 15 years the maximum term of the agreement;

Review of a boundary line adjustment at Tacoma Mall Boulevard right-of-way from 84th Street to 80th Street.

Surface Water Manager Vigoren reviewed the boundary line adjustment at Tacoma Mall Boulevard right-of-way from 84th Street to 80th Street. The reason for the adjustment is to avoid conflicts between municipalities relative to police response.

Discussion ensued if Hosmer was in the city boundaries; what is the widest width of the right-of-way near 84th street used for (it was a “turn back” area during the construction of the interchange which has turned into parking for the casino).

Rental housing safety program update.

City Attorney Wachter and Assistant City Manager for Development Services Bugher reviewed a draft ordinance on the rental housing safety program.

Discussion ensued that the ordinance provides for no rent control; and if the wording “neighborhood stability” was overreaching in the second “Whereas” clause that broadens the scope of the housing checklist.

REPORTS BY THE CITY MANAGER

City Manager Caulfield congratulated Brent Champaco for placing first for most improved City Connections newsletter by the National Association of Government Communicators.

After tomorrow’s Farmer’s Market on June 14, 2016, at 3:15 p.m., a celebratory event will be held thanking vendors for participating in the Market in the Council Chambers.

He reported that he has been working with Congressman Denny Heck on transportation funding to be set aside for rural and military communities.

He then spoke about the Cascadia Rising emergency management exercise that City staff participated in on June 8, 2016. He noted that a Washington National Guard was placed as a direct liaison in our EOC.

He then reported that JBLM are being forced to deny entry to approximately 300 persons per week because individuals do not have any real ID Act compliant credentials, the result to Washington State not putting into place the policies and procedures to ensure state drivers licenses and ID cards are compliant with federal

law. He suggested that a letter be sent to State legislators expressing concerns about this matter.

He then noted the following calendar events:

- June 14, 3:15 PM to 5:00 PM, Farmers Market Vendor Appreciation Reception, please join us in the City Council Chambers for snacks and mingling to show our appreciation to the market vendors
- June 15, 8:00 AM to 10:30 AM, Developer's Forum, McGavick Center
- June 20, 2016, 20th Anniversary Pizza with Letter-Writing Student Contestants & Jury Panel, 5:30 PM-6:30 PM, American Lake Room (Conference Room 1E)

Discussion ensued on where would the City find funding for the Alliance for South Sound Behavioral Health Coalition if the Council were to fund this capital project.

Review of a Washington State Department of Commerce grant for the South Sound Military Community Partnership to conduct a JBLM North Clear Zone Land/Property Evaluation and Relocation Study.

City Manager Caulfield noted that a \$50,000 grant was received from the Department of Commerce to conduct a study for the north clear zone.

Discussion ensued on the June 11, 2016 Joint Council and Clover Park School District Board meeting.

ITEMS TENTATIVELY SCHEDULED FOR THE JUNE 20, 2016 REGULAR CITY COUNCIL MEETING:

1. Business showcase. – Bite Me!
2. Recognizing the City's 20th Anniversary letter-writing competition winners and student art murals.
3. Approving an amendment to the agreement with Bruce Dees and Associates relative to Springbrook Park improvements. – (Motion – Consent Agenda)
4. Authorizing the execution of an agreement with the Washington State Department of Commerce, in the amount of \$50,000, relative to a JBLM North Clear Zone Land/Property Evaluation and Relocation Study. – (Resolution – Consent Agenda)
5. Awarding a bid for chip sealing city streets. – (Motion – Consent Agenda)

- 6. Granting a franchise to Astound Broadband, LLC, dba Wave, to install, operate and maintain a fiber optic-based telecommunications system within the public rights-of-way in the city of Lakewood. – (Ordinance – Regular Agenda)
- 7. Adopting the 2017-2022 Six Year Transportation Improvement Program. – (Resolution – Regular Agenda)

CITY COUNCIL COMMENTS

Councilmember Moss commented on the Farmers Market and the Army Ball event she attend. She then commented on the Air Lift Change of Command ceremony she attended/

Councilmember Barth commented on the opening of Farmers Market.

Deputy Mayor Whalen commented on his daughter’s graduation. He then commented on Commander Lanza’s event on Wednesday, June 15, 2016.

Mayor Anderson commented on the Farmers Market opening. He spoke on a meeting he attended with Western State Hospital’s new CEO. He then spoke about a meeting he attended on mental health issues with Mayor Lucas and Pierce County Councilmember Doug Richardson. He encouraged Councilmembers to encourage Congressman Denny Heck to move his office to City Hall. He noted that he will not be attending the June 20, 2016 Council meeting. He then asked if Councilmembers supported sending a letter to State legislators concerning the ID compliance issue relative to entry into JBLM. It was the consensus of the Council to proceed with preparing such a letter.

* * * * *

Executive Session

Mayor Anderson announced that Council will recess into Executive Session for approximately 20 minutes to discuss a pending litigation.

* * * * *

Council recessed into Executive Session at 9:20 p.m. and reconvened at 9:40 p.m.

ADJOURNMENT

There being no further business, the meeting adjourned at 9:40 p.m.

DON ANDERSON, MAYOR

ATTEST:

ALICE M. BUSH, MMC
CITY CLERK

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: June 20, 2016	TITLE: A Resolution to accept a Washington State Department of Commerce grant in the amount of \$50,000 for the JBLM Northern Clear Zone Land Valuations and Relocation Analysis.	TYPE OF ACTION: — ORDINANCE <input checked="" type="checkbox"/> RESOLUTION NO. 2016-11 — MOTION NO — OTHER
REVIEW: June 13, 2016	ATTACHMENTS: Draft Resolution	

SUBMITTED BY: David Bugher, Assistant City Manager for Development Services & Bill Adamson, Program Manager, South Sound Military Communities Partnership.

RECOMMENDATION: It is recommended that the City Council adopt the attached Draft Resolution accepting a grant in the amount of \$50,000.00 from the Washington State Department of Commerce.

DISCUSSION: The Washington State Department of Commerce is currently coordinating release of grant funds to the South Sound Military & Communities Partnership sponsored by the City of Lakewood. Department of Commerce is requiring the governing body, or project sponsor to accept the grant, which in this situation, is the Lakewood City Council given that the City acts as the fiscal agent on behalf of the SSMCP. *(See next page.)*

ALTERNATIVE(S): Deny or take no action on the Draft Resolution. However, such actions would be contrary with current City policy to remove encroachments from the most recently approved McChord Field Air Installation Compatibility Zone report.

FISCAL IMPACT: This is a reimbursable grant. Under this type of grant, the Department of Commerce agrees to reimburse the City for work performed and/or costs incurred up to the total amount specified in the grant (\$50,000). Such costs must be allowable in accordance with the scope of work contained in the contract documents. To summarize, Lakewood advances the expenditures, and thereafter, Lakewood receives reimbursement provided it submits proper documentation to the Department of Commerce.

Prepared by	 <hr style="width: 80%; margin: 0 auto;"/>
Department Director	City Manager Review

DISCUSSION - CONTINUED: At the end of the project, Lakewood and the SSMCP will have the information needed to seek appropriate levels of funding to purchase the private property within the Clear Zone, and a clear understanding of what is needed for commercial and industrial firms operating there to relocate within the local area and continue operations with a minimum of disruption to their businesses.

The purpose of this project is to determine the fair market value of the privately-held property within the North Clear Zone, and develop a set of alternatives for the relocation of commercial and industrial activity currently located there. The State of Washington has agreed to provide \$50,000 (must be expended no later than June 30, 2017). Pierce County has also agreed to provide \$50,000 (must be expended no later than 31 December 2016).

These funds will be combined to hire a consultant to perform the land / property valuations and relocation analyses. The City of Lakewood will act as the project sponsor and fiduciary agent for SSMCP. The SSMCP will serve as the project coordinator, and as the point of contact for consulting services.

RESOLUTION NO. 2016-11

A RESOLUTION of the City of Lakewood accepting a grant in the amount of \$50,000.00 from the Washington State Department of Commerce

WHEREAS, the 2016 legislature appropriated \$50,000.00 to the Washington State Department of Commerce for Joint Base Lewis McChord (JBLM) North Cler Zone Base Realignment and Closure Preparation; and

WHEREAS, the City of Lakewood, through the South Sound Military Communities Partnership (SSMCP) has, and continues to work with JBLM to explore options addressing the North Clear Zone; and

WHEREAS, the North Clear Zone is a 9 million square foot area of land immediately north of McChord Field, with substantial portions within the City of Lakewood; and

WHEREAS, one option towards ensuring the competitive nature of JBLM is to evaluate, via appraisal of the properties within the North Clear Zone; and

WHEREAS Pierce County has committed \$50,000.00 towards the cost of an appraisal and the City of Lakewood having recently been awarded a grant from the Department of Commerce to cover an additional \$50,000.00; and

WHEREAS these funds will be used to determine the fair market value of privately-held property within the North Clear Zone, develop a set of alternatives for the relocation of commercial and industrial activity currently located there and to develop estimates of value for properties within the NCZ and alternatives for the relocation of firms currently located within the NCZ.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON HEREBY RESOLVES, as Follows:

Section 1. The City Council accepts the grant of fifty thousand dollars (\$50,000.00) from the Washington State Department of Commerce for reimbursement for land valuation for the Joint Base Lewis McChord (JBLM) Base Realignment and Closure Preparation.

Section 2. That this Resolution shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 20th day of June, 2016.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Alice M. Bush, MMC, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: June 20, 2016	TITLE: Motion authorizing award of a construction contract to Granite Construction Company in the amount of \$250,000.00 for the construction of the 2016 Chip Seal Program.	TYPE OF ACTION: — ORDINANCE — RESOLUTION <u>X</u> MOTION NO. 2016-26 — OTHER
REVIEW:	ATTACHMENTS: Vicinity Map Bid Tabulations	

SUBMITTED BY: Don Wickstrom, P.E., Public Works Director/City Engineer.

RECOMMENDATION: It is recommended that the City Council authorize award of a construction contract to Granite Construction Company in the amount of \$250,000.00 for the construction of 2016 Chip Seal Program, City Project No. 302.0005.

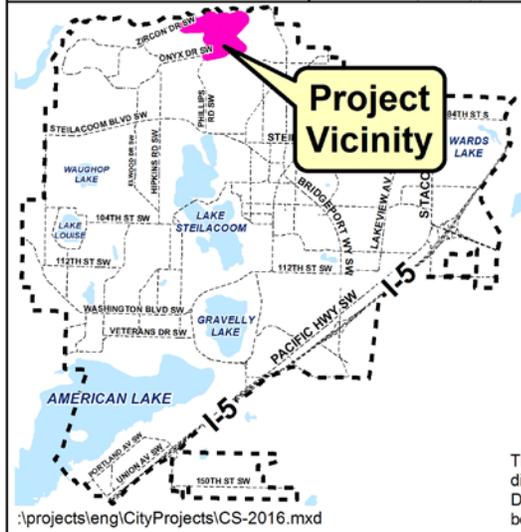
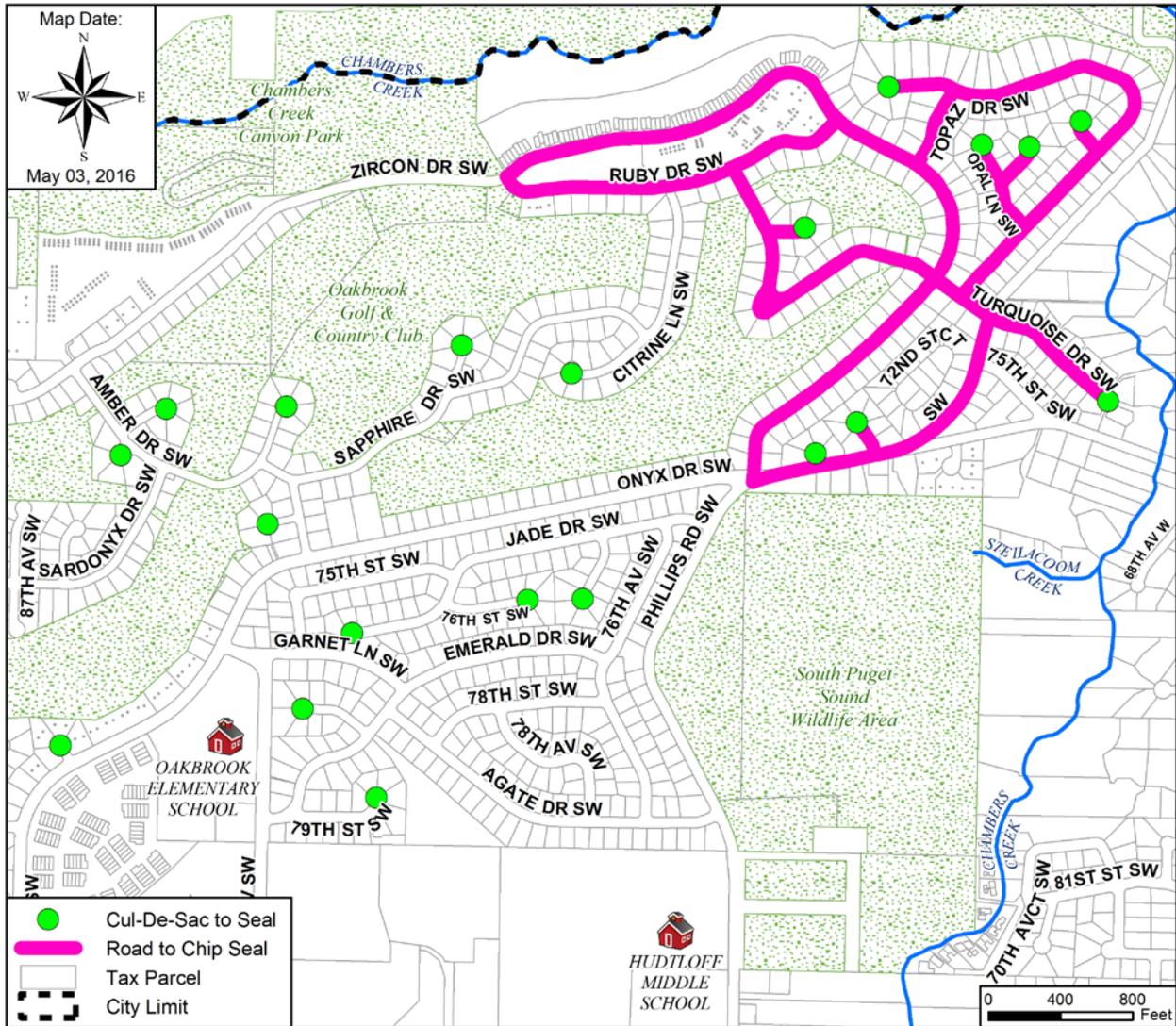
DISCUSSION: Through this project the City will construct 3.3 miles of Chip Seal and 7,400 square yards of Armorseal in the Oakbrook neighborhood.

ALTERNATIVE(S): There is no practical alternative other than to reject all bids and to not construct the project. The City received three bids; attached is the Bid Tabulations. The low bid is below the Engineer’s Estimate, it is not anticipated that rebidding the project would result in lower bids.

FISCAL IMPACT: This project is funded through the City’s 2015-2016 budget.

<hr style="border: 0; border-top: 1px solid black; margin-bottom: 5px;"/> Prepared by <hr style="border: 0; border-top: 1px solid black; margin-top: 10px;"/> Department Director	 <hr style="border: 0; border-top: 1px solid black; margin-top: 5px;"/> City Manager Review
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VICINITY MAP



City of Lakewood Public Works

2016 Chip Seal Program

This product was prepared with care by City of Lakewood GIS. City of Lakewood expressly disclaims any liability for any inaccuracies which may yet be present. This is not a survey. Datasets were collected at different accuracy levels by various sources. Data on this map may be shown at scales larger than its original compilation. Call 253-589-2489 for further information.

BID TABULATIONS

PROJECT NAME: 2016 Chip Seal Program
 PROJECT NO.: 302.0005
 BID OPENING DATE: June 14, 2016

Note: We hereby certify that these tabulated bids represent all bids received and that the additions of all prices shown have been checked and corrected.

				ENGINEER'S ESTIMATE		Granite Construction		Sierra Santa Fe		DooLittle Construction	
ITEM	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
1	Mobilization	1	LS	\$30,000.00	\$30,000.00	\$25,000.00	\$25,000.00	\$11,264.00	\$11,264.00	\$10,000.00	\$10,000.00
2	Minor Change	1	EST	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00
3	SPCC Plan	1	LS	\$500.00	\$500.00	\$500.00	\$500.00	\$1,000.00	\$1,000.00	\$500.00	\$500.00
4	Traffic Control Supervisor	1	LS	\$4,000.00	\$4,000.00	\$9,500.00	\$9,500.00	\$1,500.00	\$1,500.00	\$4,000.00	\$4,000.00
5	Project Temporary Traffic Control	1	LS	\$20,000.00	\$20,000.00	\$22,983.75	\$22,983.75	\$20,757.00	\$20,757.00	\$23,000.00	\$23,000.00
6	Portable Changeable Message Sign	2	Mon	\$6,000.00	\$12,000.00	\$2,000.00	\$4,000.00	\$1,705.00	\$3,410.00	\$2,000.00	\$4,000.00
7	Asphalt Emulsion CSS-1H Fog Seal	19	Ton	\$900.00	\$17,100.00	\$1,000.00	\$19,000.00	\$1,324.75	\$25,170.25	\$600.00	\$11,400.00
8	Asphalt Emulsion CRS-2P Chip Seal	90	Ton	\$1,200.00	\$108,000.00	\$675.00	\$60,750.00	\$636.75	\$57,307.50	\$1,055.00	\$94,950.00
9	Furnishing and Placing Crushed Screengin 3/8" to No. 10	47,500	SY	\$1.10	\$52,250.00	\$1.45	\$68,875.00	\$1.19	\$56,525.00	\$1.60	\$76,000.00
10	Armorseal A100 HD - 2 Coat Process	7,375	SY	\$5.00	\$36,875.00	\$3.00	\$22,125.00	\$3.57	\$26,328.75	\$2.80	\$20,650.00
11	Emulsion Cost Price Adjustment	1	EST	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00	\$2,000.00
12	Removal of Striping and or Type 2 RPMs	10,000	LF	\$0.20	\$2,000.00	\$0.60	\$6,000.00	\$2.36	\$23,600.00	\$0.40	\$4,000.00
13	Raised Pavement Marker Type 2	300	EA	\$9.00	\$2,700.00	\$4.00	\$1,200.00	\$15.45	\$4,635.00	\$4.00	\$1,200.00
14	Project Sign	2	EA	\$750.00	\$1,500.00	\$500.00	\$1,000.00	\$1,287.50	\$2,575.00	\$1,200.00	\$2,400.00
15	Paint Line	9,975	LF	\$0.50	\$4,987.50	\$0.35	\$3,491.25	\$0.84	\$8,379.00	\$0.34	\$3,391.50
16	Stop Bar	315	SF	\$18.00	\$5,670.00	\$5.00	\$1,575.00	\$26.56	\$8,366.40	\$5.00	\$1,575.00
					\$301,582.50		\$250,000.00		\$254,817.90		\$261,066.50

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: June 20, 2016	TITLE: Amending Contract # 2015-246 with Bruce Dees and Associates for Springbrook Park Improvement Project	TYPE OF ACTION: — ORDINANCE NO. — RESOLUTION NO. <u>X</u> MOTION NO. 2016-27 — OTHER
REVIEW:	ATTACHMENTS: Contract Amendment Exhibit A – Summary and Scope of Work	

SUBMITTED BY: Mary Dodsworth, Parks, Recreation and Community Services Director

RECOMMENDATION: It is recommended that the City Council authorize the City Manager to sign a contract amendment with Bruce Dees and Associates which adds the services necessary to complete the Springbrook Park Improvement Project.

DISCUSSION: In October the City contracted with Bruce Dees and Associates to do the design work associated with the Springbrook Park Improvement Project. Since that time the City has added a pedestrian bridge to the project and expanded the design to include the property to the east of the park (purchased with Conservation Futures funds). This created additional work for the consultant and several additional subconsultants. The original contract agreement did not include fees for construction management as we were waiting to see if the new grants elements would be funded. Attached is Exhibit A outlining the additional charges and credits to the project.

ALTERNATIVE(S): Council could not approve the changes or divide the changes or project construction into separate contract amendments or phases. This could delay the overall construction timeline and reduce any project efficiencies created by combing the work.

FISCAL IMPACT: Fiscal impact is \$ 53,113. Funds from the various project grants and donations will be used to cover the additional design and project management costs.

_____ Prepared by _____ Department Director	 _____ City Manager Review
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**AMENDMENT NO. 1
TO Bruce Dees and Associates
FOR Springbrook Park Development Project (Contract # 2015-246)**

This Amendment ("Amendment No. 1") is made between the City of Lakewood, a Washington municipal corporation ("City"), and Bruce Dees and Associates, a Washington State Limit Liability Corporation ("Contractor"). The City and Contractor (together "Parties"), for valuable consideration and by mutual consent of the parties, agree to amend the original Agreement for additional design and planning services ("Agreement") dated effective _____, as follows:

1. AMENDED COMPENSATION. The amount of compensation, as referenced by Section 4 of the Agreement, shall be amended to change the total compensation the City shall pay the Contractor and the rate or method of payment, as delineated in Exhibit "A", attached hereto and incorporated by this reference. The Contractor agrees that any hourly or flat rate charged by it for its services contracted for herein shall remain locked at the negotiated rate(s) for the Term. Except as otherwise provided in an attached Exhibit, the Contractor shall be solely responsible for the payment of any taxes imposed by any lawful jurisdiction as a result of the performance and payment of this Agreement.

2. GENERAL PROVISIONS. All other terms and provisions of the Agreement, together with any prior amendments thereto, not modified by this Amendment, shall remain in full force and effect. Any and all acts done by either Party consistent with the authority of the Agreement, together with any prior amendments thereto, after the previous expiration date and prior to the effective date of this Amendment, is hereby ratified as having been performed under the Agreement, as modified by any prior amendments, as it existed prior to this Amendment. The provisions of Section 4 of the Agreement shall apply to and govern this Amendment. The parties whose names appear below swear under penalty of perjury that they are authorized to enter into this Amendment, which is binding on the parties of this contract.

IN WITNESS, the Parties hereto have caused this agreement to be executed the day and year first above written.

Date: _____

CITY OF LAKEWOOD

BRUCE DEES AND ASSOCIATES

John J. Caulfield, City Manager

Bruce Dees, Principal

ATTEST:

Alice M. Bush, MMC, City Clerk

APPROVED AS TO FORM:

Heidi Ann Wachter, City Attorney

**BRUCE
DEES &
ASSOCIATES** LLC

Landscape Architecture • Urban Design
Site Planning • Recreation Facilities Design

June 9, 2016

Ms. Mary Dodsworth
City of Lakewood
6000 Main Street SW
Lakewood, WA 98499

Job No.: 146-03-03

RE: Springbrook Park Improvements
Professional Services Agreement
Contract No: 2015-246
Amendment #1

Dear Mary,

The Springbrook Park project will be ready to advertise for bidding as soon the existing buildings are removed and permits issued.

When we started the project, available funds for the construction were severely limited. We, however, provided the design for all elements. Some of that work required additional subconsultant work that we did not anticipate. Now that additional funds are available we are requesting compensation for the additional design work. Following is a description of the additional subconsultant work.

ADDITIONAL SUBCONSULTANT WORK

IRRIGATION

Improving the irrigation at the existing field was not in the original scope of work, but it was requested that we design the necessary improvements for proper operation of the system. The existing irrigation static pressure was found to be too low for proper coverage of the existing system and to operate an expanded system without a booster pump. Accordingly, we needed to retain an irrigation consultant to design a booster pump and irrigation system.

Additional Fees Requested: **\$4,600.00**

STORM DRAINAGE

In our presubmittal meeting with the city we learned that the project would be required to extend the existing sidewalk and curb to the east property line and because that required work is in the right of way, a civil engineer had to be retained to seal the drawing and provide a drainage report.

Additional Fees Requested: **\$11,500.00**

ELECTRICAL DESIGN

During the course of design an electrical service was requested for the shelter. That service and service to the booster pump required that we retain an electrical engineer.

Additional Fees Requested: **\$3,450.00**

STRUCTURAL ENGINEER

When we developed the original scope of work, we anticipated using a pre-manufactured bridge with the intent that the manufacturer would provide the design for the footings as well as the bridge. During the course of the design, it became apparent that the bridge needed to be extended to span both the creek and the drainage ditch on the north which then required special design for the footings. That design required that we retain a structural engineer.

Additional Fees Requested: **\$2,576.00**

ENVIRONMENTAL REVIEW

We did have fees in for an environmental review and wetland delineation for the bridge crossing, the city has not required this review, therefore these proposed fees will be credited back.

Credit: **-\$2,300.00**

SUBCONSULTANT FEE SUMMARY

Royal Waldock	Irrigation Designer	\$ 4,600.00
Sitts & Hill	Civil Engineer	\$11,500.00
Stantec Consulting	Electrical Engineer	\$ 3,450.00
Eric Fisher	Structural Engineer	\$ 2,576.00
Subtotal		\$22,126.00
Soundview Consulting	Environmental Consultant	-\$ 2,300.00
Total		\$19,826.00

Total Additional Subconsultant Fee Requested: \$19,826.00

ADD BIDDING AND CONSTRUCTION REVIEW

As you know when our contract was executed it was only for schematic design through construction document phase. At that time, it was agreed that bidding and review would be added to our contract when the project went into the bidding and review phases. Now that we are about to begin bidding and review we request that the fees be increased to include bidding and review.

BIDDING

The following is the scope for bidding assistance and construction review:

Bidding Assistance

A. Begin Advertisement

Bruce Dees & Associates will write the advertisement for the bid. The Client will be responsible for advertising costs.

B. Coordination During Bidding Phase

1. Answer contractor questions.
2. Conduct an on-site pre-bid conference.
3. Issue addendums if required.

Note: All printing costs for bidding will be paid for by the Client.

C. Review Bids, and Pre-Construction Coordination.

1. Review all bid proposals.
2. Review contractor qualifications and provide written recommendation award of contract.
3. Conduct a pre-construction conference with the contractor and the Client or as required.

D. Products

1. Complete construction documents ready for advertisement, reproducibles, and disk of the drawings, and specifications.
2. Final construction cost estimate.

Bidding Assistance Fee Requested: \$3,234.00

Construction Review

A. Construction Review

We will provide weekly construction review. It is anticipated that construction will be completed within a 3-month period (12 site visits). Construction review will include:

1. Review for approval or disapproval materials and equipment submittals.
2. Conduct periodic visits to observe the progress and quality of the work in conformance with the contract drawings.
3. Issue instructions to the Contractor, respond to Contractor RFI's and prepare field directives and change orders, if applicable.
4. Keep the Client advised of work in progress.
5. Conduct weekly construction progress meetings. Generate and issue minutes to appropriate parties.
6. Review contract payment requests.

B. Project Close-Out

1. Conduct a final review and prepare punchlist.
2. Conduct a final review of punchlist items and recommend acceptance.
3. Provide record drawings from contractor "as-built" prints.
4. All project close-out documents such as guarantees, warranties, and legal documents will be assembled in a bound notebook. A record of the entire construction process will be included in this document.
5. Conduct a one-year warranty review.

C. Deliverables

1. Punchlist and back check.
2. Operations & Maintenance Manual.
3. Disk of Asbuilt Drawings in PDF.
4. One set of Asbuilt Drawings.
5. One Year Warranty review letter.

Construction Review Fee Requested: \$14,553.00

EXPANDED MACC (MAXIMUM ALLOWABLE CONSTRUCTION COST)

We provided design for all elements requested, even though the estimated construction cost exceeded the MACC (the MACC being the basis for our fee). Now that additional funds have been acquired, and therefore the MACC has been increased we are requesting that our fees be adjusted accordingly. Our agreement states:

"if during the course of the project, the client elects to expand the scope of work or increase the MACC resulting in additional work, designs fees for the additional work will be negotiated"

Funds added to the MACC \$155,000.00 x 10% = \$15,500.00

At this time we request that our agreement be amended to include \$15,500.00 for all work from schematic design through construction review.

Expanded MACC Fee Requested: \$15,500.00

SUMMARY

We request that our contract be amended to include the additional subconsultant fees, bidding assistance and construction review and additional design fees for the increased MACC.

\$19,826.00	Additional Subconsultant Fees
\$ 3,234.00	Bidding
\$14,553.00	Construction Review
<u>\$15,500.00</u>	Expanded MACC
\$53,113.00	Requested Amendment

CONTRACT SUMMARY

\$44,465.00	Original Contract
<u>\$53,113.00</u>	This Request – Amendment #1
\$97,578.00	New Contract Total

If the above meets with your approval, we request that our agreement be amended to include **\$53,113.00** for a new contact total of **\$97,578.00**.

Sincerely,

BRUCE DEES & ASSOCIATES



Bruce Dees, FASLA
Principal

ABD:sas



LANDMARKS & HERITAGE ADVISORY BOARD

April 28, 2016 Meeting Minutes
Third Floor Mt. Rainier Room
6000 Main Street SW
Lakewood, WA 98499

"The mission of the City of Lakewood Landmarks and Heritage Advisory Board is to preserve, protect and promote the unique heritage and historic resources of the City of Lakewood."

CALL TO ORDER

Presiding officer, Ms. Stephanie Walsh, Chair, called the meeting to order at 6:00 p.m.

MEETING PROTOCOL

Roll Call

Landmarks & Heritage Advisory Board Members Present: Stephanie Walsh, Chair; Beth Campbell, Joan Cooley, Bob Jones, Dennis Dixon and Bill Harrison

Landmarks & Heritage Advisory Board Members Excused: Glen Spieth, Vice-Chair

Landmarks & Heritage Advisory Board Members Absent: Walter Neary

Council Liaison to LHAB: Councilmember John Simpson

Youth Council Liaison to LHAB: Cassie Daniels

Landmarks & Heritage Advisory Board Consultant: Jennifer Schreck

Staff Present: Ramon Rodriguez, Assistant Planner; and Karen Devereaux, Administrative Assistant

Acceptance/Changes to Agenda

Ms. Stephanie Walsh, Chair, queried the group for any needed changes to the agenda. Ms. Walsh suggested adding a discussion of the Pierce County Landmarks Preservation Grant under New Business.

Ms. Joan Cooley made the motion to accept the change. Ms. Beth Campbell seconded the motion. A voice vote was taken and the motion carried unanimously.

Approval of Minutes

Ms. Stephanie Walsh, Chair, opened the floor asking if there were any additions, changes or modifications to the February 25, 2016 or March 24, 2016 meeting minutes. **Ms. Beth Campbell made the motion to approve both sets of minutes as written. The motion was seconded by Mr. Dennis Dixon. A voice vote was taken and the motion carried unanimously.**

Announcements

Ms. Jennifer Schreck, Consultant, provided a poster from the Department of Archeology + Historic Preservation celebrating the 50th Anniversary of the signing of the National Historic Preservation Act for staff to display inside City Hall.

PUBLIC COMMUNICATIONS

None

UNFINISHED BUSINESS

2016 Grant Application Project

Ms. Schreck informed the group the CLG application was submitted on time before the April 22nd deadline. Interviews of possible recipients could be scheduled as early as next month with a decision and award being announced in June 2016. Ms. Schreck will notify the board members when she receives any news.

Distribution of Touring Map and Benefits Brochures

Ms. Stephanie Walsh agreed to contact Ms. Kim Prentice, Community Relations Director, Clover Park School District, to discuss distribution of the touring map to Washington State history students for summer classes or in the coming school year 2016-2017.

Clock Update

Mr. Bob Jones shared with members his clock on the Colonial Center now has power and he has secured a repairman to assess its current condition and future maintenance. Mr. Jones noted that Mr. Glen Spieth informed him he has contacted the owner of the FedEx building at Gravelly Lake and Motor Avenue SW but hasn't received any further response regarding possibility of saving the clock for future uses after building is demolished.

NEW BUSINESS

Set Date for Strategic Planning Retreat

Using the timeline within the CLG grant application board members agreed to hold a strategic planning retreat on Saturday, January 28, 2017 from 9:00 a.m. to 1:00 p.m. in the Mt. Rainier Conference Room at City Hall. They subsequently have chosen to cancel the regularly scheduled monthly meeting on Thursday, January 26, 2017.

Application for Designations

Ms. Joan Cooley suggested a simplified form be used for the community landmark designation application. Currently, the more exhaustive full-historic designation application is used for this less significant designation. Mr. Bob Jones offered to edit the longer application to use with the simpler designation. Mr. Dennis Dixon also offered to assist in the project. Ms. Schreck offered to forward the word document to the sub-committee consisting of Mr. Jones, Mr. Dixon and Ms. Cooley to accomplish the task of a shorter, more user-friendly application.

Pierce County Landmarks Preservation Grant Opportunity

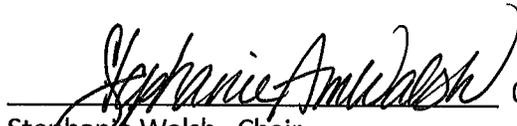
Mr. Dennis Dixon explained to board members the available grant allowing for historic preservation of history related project proposals and does require a 50% match of funds. Mr. Dixon suggested members visit the website to get a better idea of the kinds of projects supported over the years. Members began to discuss additional ideas for efforts to preserve the Little Red Schoolhouse. Ms. Walsh agreed to contact CPSD Superintendent, Debbie LeBeau, to query status of their project.

PUBLIC HEARING

None

Next Meeting is scheduled for Thursday, May 26, 2016 at 6:00 p.m. in Executive Conference Room 3A.
(The board members agreed to reschedule the May meeting to June 2, 2016).

Meeting Adjourned at 7:05 p.m.


06/02/2016
Stephanie Walsh, Chair
Landmarks & Heritage Advisory Board


06/02/2016
Karen Devereaux, Recording Secretary to the
Landmarks & Heritage Advisory Board

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED June 20, 2016	TITLE: Astound Franchise Agreement	TYPE OF ACTION: <input checked="" type="checkbox"/> ORDINANCE NO. 643
REVIEW: June 14, 2016	ATTACHMENTS: Ordinance	<input type="checkbox"/> RESOLUTION NO. <input type="checkbox"/> MOTION NO. <input type="checkbox"/> OTHER

SUBMITTED BY: Adam Lincoln, Assistant to the City Manager

RECOMMENDATION: It is recommended that the City Council authorize the execution of a franchise agreement between the City of Lakewood and Astound.

DISCUSSION: Astound Broadband LLC seeks to establish a franchise agreement with the City of Lakewood. At this time, Astound is only interested in establishing commercial internet services within the City. The scope of the franchise agreement allows for the establishment of a dark fiber network, which is meant for private commercial telecommunication uses and is similar to the type of network that is allowed for in the Zayo franchise agreement that was established with the City in 2013. The initial project that Astound will be working on is to establish a connection to the Lakewood Library. Astound is not offering any cable television services and should it offer such services the City would require a new franchise agreement, similar to the Comcast agreement that was adopted by the City Council last year.

Continued below.

ALTERNATIVE(S): The City may choose to further negotiate or not pass this Franchise Agreement Ordinance.

Prepared by	 _____ City Manager Review
Department Director	

Astound seeks to establish a franchise agreement with the City of Lakewood so that it can then create a dark fiber network for commercial use. The City of Lakewood has two similar agreements with Integra (2009) and Zayo (2013). The agreement allows for Astound to create a network that may be used for telecommunications purposes but may not be used for cable television services. This franchise agreement establishes specific guidelines that Astound will follow when working within the City right of way and for the imposition of a utility tax.

Term and extensions: 10 year agreement with a potential 5 year extension.

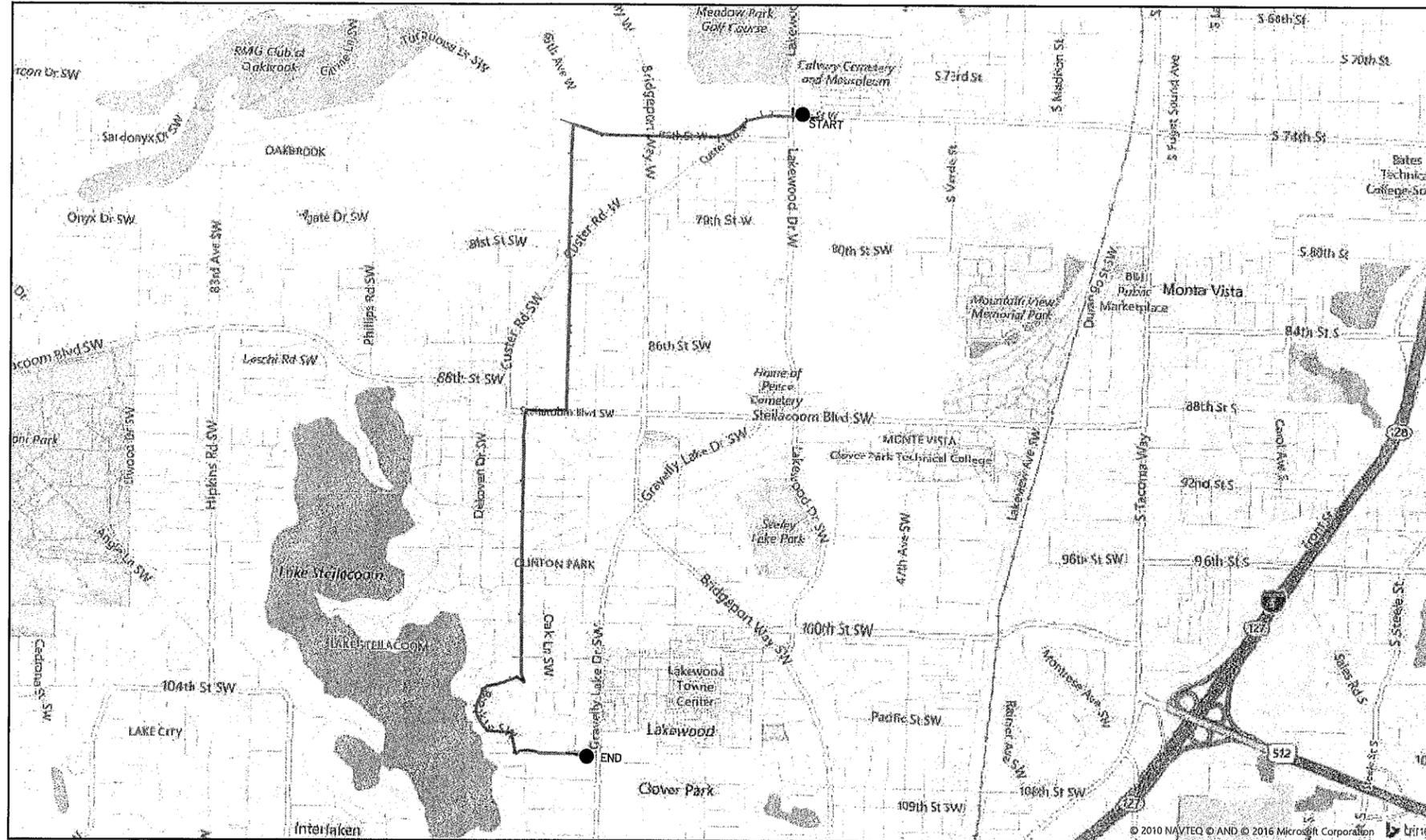
Franchise fees and other fees: The Federal Telecommunications Act does not allow the City to pursue any franchise fee with Astound unless it offers more than telecommunications services.

Utility Tax: Maintains existing utility tax at 6%. There is no revenue estimate at this time. Astound has indicated that projects being planned at this time are fairly small in scope. The Lakewood Library project is the only project planned at this time.

Street maintenance and undergrounding: The Agreement allows Astound to have access to the City right of way. Astound is then required to follow the applicable permit processes to conduct work and is required to restore all streets to the original state it was found in. The Public Works Director has the authority to sign off on the quality of the repairs.

The Agreement includes language for undergrounding utilities. The Agreement does not require all of the fiber lines to be undergrounded, but it does state that if utilities in the area are already undergrounded and Astound chooses to run new line, they will also place their lines underground. In joint trenching situations, Astound will share in the costs of such an endeavor and will participate with other providers in any joint trenching projects to relocate overhead wiring to an underground facility.

ADDRESS: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499
 PROJECT NAME: 7484 - PCLS LAKEWOOD LIBRARY



SITE LOCATION

BILL OF MATERIALS

ITEMS	QUANTITY	UNITS	FURNISHED BY	PLACED BY	COMMENTS
FIBER AERIAL	17969	FT.			
FIBER STORAGE	5400	FT.			
1/4" EHS STRAND	18678	FT.			

CONTACTS

WAVE CONTACT:
 CHRISTOPHER MANTLE
 401 KIRKLAND PARKPLACE, SUITE # 500
 KIRKLAND, WA 98033
 C. 425.466.3245
 CMANTLE@WAVEBROADBAND.COM

BRYSON BAILY
 MGC TECHNICAL CONSULTING, INC.
 PO BOX 332
 BOTHELL, WA 98041
 C.206.799.3001
 BRYSON@MGCTECHNICAL.COM

SHEET INDEX

1. COVER SHEET / SITE LOCATION
2. LEGEND
3. GENERAL NOTES
4. PLAN VIEW A
5. TYPICALS
6. SITE PHOTOGRAPHS

SCOPE OF WORK:

FROM A PROPOSED SPLICE CASE ON THE SOUTH SIDE OF 74TH ST W, INSTALL FIBER CABLE USING AERIAL AND UNDERGROUND CONSTRUCTION WEST ON 74TH ST W, SOUTH ON CUSTER RD W, WEST ON 75TH ST W, SOUTH ON JOHN DOWER RD W, WEST ON STEILACOOM BLVD SW, SOUTH ON MEADOW RD SW, SOUTH ON BROOK LN SW AND EAST ON WILDAIRE RD SW TO AN EXISTING VAULT PLACED BY OTHERS ON THE SOUTH SIDE OF WILDAIRE RD SW.



Know what's below.
 Call before you dig.



0 1250' 2500'

SCALE: 1"=2500'

3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.

PROJECT NUMBER: 7484

LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499

DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg

CONFIDENTIAL/PROPRIETARY SHEET 048 22

LEGEND

LINETYPES

	UG FIBER - EXISTING
	UG FIBER - PROPOSED
	AERIAL FIBER - EXISTING
	AERIAL FIBER - PROPOSED
	STRAND - EXISTING
	STRAND - PROPOSED
	CONDUIT - EXISTING
	CONDUIT - PROPOSED
	INNERDUCT - EXISTING
	INNERDUCT - PROPOSED
	GAS
	WATER
	TELEPHONE
	FIBER OPTIC
	ELECTRIC
	SANITARY SEWER (SEW)
	STORM DRAIN
	FENCE
	CABLE TV
	STEAM
	OIL
	UNKNOWN UTILITY
	RIGHT OF WAY
	EDGE OF PAVEMENT

SYMBOL DESCRIPTION

ASW	ASPHALT SIDEWALK
BIP	BLACK IRON PIPE
BSP	BLACK STEEL PIPE
CSW	CONCRETE SIDEWALK
EOP	EDGE OF PAVEMENT
EOTW	EDGE OF TRAVEL WAY
FOC	FACE OF CURB
HDPE	HIGH DENSITY POLYETHYLENE
HH	HANDHOLE
JB	JUNCTION BOX
MH	MANHOLE
MP	MILE POST
O/S	OFFSET
PR	POWER RISER
PVC	POLY VINYL CHLORIDE
RGS	RIGID GALVANIZED STEEL CONDUIT
ROW	RIGHT OF WAY
STA.	STATION

	RISER
	TELEPHONE
	POWER VAULT
	CATCH BASIN/INLET
	FIRE HYDRANT
	TRANSMISSION/DISTRIBUTION
	TRANSMISSION
	DISTRIBUTION
	GROUND/BOND
	STREET LIGHT
	TREE
	CULVERT
	WING WALL
	BRIDGE
	MISC. UTILITY

	UTILITY POLE - EXISTING
	POLE - PROPOSED
	HANDHOLE - EXISTING
	HANDHOLE - PROPOSED
	MANHOLE - EXISTING
	MANHOLE - PROPOSED
	PULLBOX - EXISTING
	PULLBOX - PROPOSED

	VAULT - EXISTING
	VAULT - PROPOSED
	AERIAL STORAGE - EXISTING
	AERIAL STORAGE - PROPOSED
	VAULT/BUILDING STORAGE - EXISTING
	VAULT/BUILDING STORAGE - PROPOSED
	POLE ANCHOR/DOWN GUY - EXISTING
	POLE ANCHOR/DOWN GUY - PROPOSED
	DOWN GUY TO EXISTING ANCHOR - PROPOSED

	TERMINATION - EXISTING
	TERMINATION - PROPOSED
	BUILDING CALLOUT - PROPOSED

	MANUFACTURER NAME	SPLICE POINT - EXISTING
	MANUFACTURER NAME	SPLICE POINT - PROPOSED
		SEQUENTIAL CALLOUT
		SEQUENTIAL IN TAIL CALLOUT
		SEQUENTIAL TAIL OUT CALLOUT

	POLE ATTACHMENT CALLOUT - EXISTING USE DYNAMIC PULL DOWN TO SELECT FROM 1 TO 6 ATTACHMENTS
	POLE ATTACHMENT CALLOUT - PROPOSED USE DYNAMIC PULL DOWN TO SELECT FROM 1 TO 6 ATTACHMENTS
	CABLE FIBERS: FIBERS CABLE OWNER: OWNER CABLE LENGTH: LENGTH NOTES: CABLE SPAN CALLOUT - EXISTING FOR USE ON PAPER SPACE (SHOWN AT 50X)
	CABLE FIBERS: FIBERS CABLE OWNER: OWNER CABLE LENGTH: LENGTH NOTES: CABLE SPAN CALLOUT - PROPOSED FOR USE ON PAPER SPACE (SHOWN AT 50X)
	CONDUIT OWNER: OWNER CONDUIT LENGTH: LENGTH CONDUIT QTY: CONDUITS CONDUIT SIZE: SIZE CONDUIT TYPE: TYPE INNER DUCT QTY: INNERDUCTS INNER DUCT SIZE: SIZE INNER DUCT TYPE: TYPE NOTES: CONDUIT CALLOUT - EXISTING FOR USE ON PAPER SPACE (SHOWN AT 50X) WITH OR WITHOUT INNER DUCT INFO
	CONDUIT OWNER: OWNER CONDUIT LENGTH: LENGTH CONDUIT QTY: CONDUITS CONDUIT SIZE: SIZE CONDUIT TYPE: TYPE INNER DUCT QTY: INNERDUCTS INNER DUCT SIZE: SIZE INNER DUCT TYPE: TYPE NOTES: CONDUIT CALLOUT - PROPOSED FOR USE ON PAPER SPACE (SHOWN AT 50X) WITH OR WITHOUT INNER DUCT INFO
	STRAND TYPE: TYPE STRAND LENGTH: LENGTH NOTES: STRAND CALLOUT - EXISTING FOR USE ON PAPER SPACE (SHOWN AT 50X)
	STRAND TYPE: TYPE STRAND LENGTH: LENGTH NOTES: STRAND CALLOUT - PROPOSED FOR USE ON PAPER SPACE (SHOWN AT 50X)



3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT
WAVE ENGINEER: CHRISTOPHER MANTLE				
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY SHEET 049 22				

GENERAL NOTES

GENERAL NOTES:

The locations of utilities shown on these drawing are only approximate. MGC TECHNICAL CONSULTING, INC. hereby disclaims any responsibility to third parties for the accuracy of this information. Persons working in the area covered by this drawing must contact the statewide Call-Before-You-Dig System to ascertain the location of underground utilities prior to performing any excavation.

- ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF UTILITY IMPROVEMENTS SHALL MEET OR EXCEED SITE WORK STANDARDS AND THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE CITY OF LAKEWOOD REGULATIONS AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK WITHIN PUBLIC R.O.W. OR EASEMENTS MAY REQUIRE INSPECTED AND APPROVED BY THE CITY OF LAKEWOOD INSPECTOR. INSPECTION SERVICES AND CONSTRUCTION CERTIFICATION TO BE PROVIDED BY DESIGNEE OF PROJECT SPONSOR/OWNER.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM ALL APPLICABLE AGENCIES. THE CONTRACTOR SHALL NOTIFY THE CITY OF LAKEWOOD INSPECTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY OR CONSTRUCTION ON ANY AND ALL PUBLIC IMPROVEMENTS IF REQUIRED.
- THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE CITY OF LAKEWOOD AND ALL UTILITY COMPANIES WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION, TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION, AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND ONE (1) COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB ON-SITE AT ALL TIMES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO: EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
- IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY.
- ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARD, UNLESS SPECIFICALLY STATED OTHERWISE.
- FOR WORK AFFECTING PUBLIC ROADWAYS OR IF REQUIRED BY THE CITY OF LAKEWOOD, THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL AND PHASING PLAN IN ACCORDANCE WITH M.U.T.C.D. FOR APPROVAL. PRIOR TO ANY CONSTRUCTION ACTIVITIES WITHIN OR AFFECTING THE RIGHT-OF-WAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY SAID PLANS. PRIOR TO INSTALLATION A PRECONSTRUCTION CONFERENCE SHALL BE HELD WITH CITY OF LAKEWOOD.
- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, CONSTRUCTED, REMOVED OR RELOCATED UNLESS SPECIFICALLY NOTED OTHERWISE.
- PER AGENCY STANDARDS THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING RECORD INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE AND AVAILABLE TO THE CITY OF LAKEWOOD INSPECTOR AT ALL TIMES.
- DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. FOR ADDITIONAL INFORMATION CONTACT THE ENGINEER FOR CLARIFICATION AND NOTE ON THE RECORD DRAWINGS.
- ALL EROSION AND SEDIMENT CONTROL (E.S.C.) MEASURES SHALL BE INSTALLED AT THE LIMITS OF CONSTRUCTION PRIOR TO GROUND DISTURBING ACTIVITY. ALL E.S.C. MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREAS ARE STABILIZED WITH HARD SURFACE OR LANDSCAPING.
- ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY IS SUBJECT TO THE JURISDICTION OF THE CITY OF LAKEWOOD ENGINEERING DEPARTMENT STANDARD DETAILS AND SPECIFICATIONS.
- ALL CONSTRUCTION OPERATIONS, INCLUDING THE WARMING UP, REPAIR, ARRIVAL, DEPARTURE OR RUNNING OF TRUCKS, EARTH MOVING EQUIPMENT, CONSTRUCTION EQUIPMENT AND ANY OTHER ASSOCIATED EQUIPMENT SHALL GENERALLY BE LIMITED TO THE TIME PERIOD APPROVED BY THE CITY OF LAKEWOOD.

- * A copy of the permit shall be on site at all times.
 - * All Where permits from state, county, city, railroad or government agencies are required all work performed will comply with these agencies standard specifications.
 - * Work areas to comply with OSHA/WISHA safety standards and traffic control plans approved in the permit.
 - * WDH reserves the right to inspect at any time the work being performed by the contractor. If the WDH representative determines that the work is not being performed to minimum quality standards, then the contractor shall provide rework at his own cost.
 - * Any change in project scope, including but not limited to, conduit length, conduit path, aerial path etc., must be approved by WDH representative.
 - * All footages are based on field gathered measurements, as such some variance should be anticipated. Adjust as needed.
- General Underground Construction Notes**
- * Contractor is responsible for locating all utilities 48 hours prior to construction activity. See local directory for contact telephone number of local call before you dig agency.
 - * Any and all underground utilities encountered are the responsibility of the contractor to locate, protect, and repair, if damaged.
 - * Bore depths shall be a minimum of 36" deep. If 36" depth is not attainable, you must get the written approval of the WDH Construction Lead/Manager and document, in detail, the path and depth variance.
 - * Conduit sweeps at the sides of structures (buildings or poles) shall be Sched 40 PVC 3ft radius with bell ends down.
 - * Sweeps at poles shall be a minimum of 5" from the pole and/or line up with an empty spot on any existing riser brackets
 - * If no existing riser brackets are present, the conduit shall sweep up on the side of the pole with the 'butt gain' cut out.
 - * Sweeps at the side of a building shall be approx 2" from the wall to accommodate uni-strut to secure the conduit to the wall
 - * BRC fiber sticker 'flex posts' shall be installed along the bore path per the following criteria.
 - o Flex posts shall be place when bore path is 10ft or more from road edge or back of sidewalk in non urban areas
 - o Flex posts placement intervals
 - The first post shall be visible from the riser and the second post, BRC vault or the next riser (which ever s placed).
 - The second post shall be visible from the first post and third post or BRC vault or BRC riser (which ever is placed)
 - * All vaults shall be set on a 4" layer of drainable gravel material and conduit shall extend above the gravel surface.
 - * All vaults/handholes to be equipped with security lids with locking bolts.
 - * Conduit shall extend no more than 2" above gravel base inside of vaults or no more than 2" inside vault sidewall penetrations
 - * Concrete vault wall penetrations shall be grouted, inside and out
 - * When tying into an existing conduit a 'Y' or a 'T' must be used
 - * Proof conduit and install a Mule Tape pulling line and cap/plug to prevent earth materials from entering prior to placement of fiber
 - * Work area to be restored to 'like new' conditions (or better)
 - * Conduit ends shall only terminate inside of WDH or customer vaults / buildings. Unless specifically directed otherwise.
 - * Hard surface restoration (asphalt or concrete) must be restored within 3 weeks of job completion, or as weather permits
 - * All buried infrastructure must be accompanied with warning tape and a tracer wire
 - * Locate wire to be exposed at the base of any riser
 - * Locate wire in vaults to be tied off on vault rim for access without having to get inside vault
 - * All conduit to be plugged with expandable foam or duct seal after fiber is placed
 - * Install additional pulling line for future at time of fiber placement
 - * Bore depth profile shall include
 - Detailed description of 0.00 start station. including GPS coordinates (can be obtained from Google Earth in Degrees/Decimal format
 - Bore direction in Degree's-magnetic (can be obtained from Google Earth measuring tool
 - Bore vertical profile data at 10ft intervals with occasional detailed description of interval station, listed as x+xx ft and xx in dee
 - Lateral alignment in relation to the road curb or centerline listed as 0.00ft E-W-N or S of Curb, Fogline or Centerline
- General Aerial Construction Notes**
- * All work to be NESC compliant
 - * Complete all make ready called for by SnoPUD/PSE and WDH in the Contractor Notes
 - * Install strand, anchors and guying per SnoPUD/PSE requirements and WDH construction guidelines
 - * Install anchors before strand is tensioned and fiber installed
 - * All fiber sheaths will be double lashed
 - * All WDH poles shall be installed with the 'butt gain' cut out, opposite from vehicular traffic
 - * Pole setting depth shall be at least 15% of pole length
 - * The top of the pole shall be NESC clearance complian
 - * All risers shall be installed on the 'cut out' side of the pole where the 'Butt Gain' notch i
 - * All risers to have BRC/WDH riser stickers in plain view
 - * Conduit on risers shall always have the bell end down
- Fiber Placement Notes**
- * If WDH is co-occupying developer installed conduit with other Telcom facilities, we shall pull in a multi cell duct divider system, such as MaxCell and pull the WDH fiber in one of the cells
 - * Fiber optic identification tags shall be placed at every work pole, manhole, fiber, storage loops, splice cases and where indicated by project manager. Tags need to be placed so as to be read from the ground.
 - * All fiber in vaults and exposed in buildings shall have BRC/WDH 'ID Carrots' at input and output and at approx 60ft interval
 - * Cable lubrication shall be used to reduce pulling tension. Lubricants must not contain any petroleum products
 - * A 100 foot fiber coil must remain in all vaults unless otherwise noted
 - * All risers will be capped at all times.
 - * All slack stored in vaults shall be marked with colored tape at the 'ID carrots' to indicate direction to the next structure (vault or pole, NOT snowshoe)
 - The Center of the slack shall have the colored tape next to each other to indicate the center of the stored slack
 - RED tape for the next structure South - 2 bands RED and West - 1 band RED from current location
 - BLUE tape for next structure North - 2 bands BLUE and East - 1 band BLUE from the current location
 - * When preparing slack for a new splice closure, mark the center with colored tape to show direction
 - * If there is traffic control required at the splice location, please prep the splice tails for our splice
 - * If excess cable is pulled off the reel, NEVER cut the excess off. Always distribute the excess along the project path as follows
 - Under 60ft - store at the nearest slack storage
 - Over 60ft - call the PM to determine how to distribute
 - Private property;
 - * Coordinate with site contact at least 24hrs prior to starting any work on private property
 - * Cover customer equipment to prevent contamination from construction debris



Know what's below.
Call before you dig.

3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT
 				
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ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY				SHEET 1 OF 22

CONSTRUCTION NOTES

- 1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- 2 STRAND ONLY ALONG EXISTING UTILITY POLES.
- 3 WAVE SPlice CASE PROPOSED ON PROJECT # 7486.
- 4 PROPOSED WAVE SPlice CASE.

POLE NUMBER	TP29118
SECONDARY	32'1"
SL	30'7"
SECONDARY DRIP	30'3"
SL DRIP	30'0"
SL SUPPORT	28'4"
CLICK	26'8"
ZAYO	25'3"
COMCAST ARM	24'2"
WAVE ATTACH	26'0"

POLE NUMBER	TP29117
SECONDARY DRIP	25'10"
SECONDARY	25'7"
NEUTRAL	25'3"
SECONDARY PWR RISER	25'0"
SL DRIP	23'4"
SL	23'3"
SL SUPPORT	21'1"
CLICK	20'10"
CLICK	20'10"
COMCAST	19'9"
COMCAST	19'5"
WAVE ATTACH	20'4"

POLE NUMBER	TP45338
OHG PWR 1	26'11"
OHG PWR 2	25'10"
OHG CLICK	20'7"
WAVE ATTACH	20'0"

POLE NUMBER	TP29115
SECONDARY	26'0"
SL	23'0"
SL DRIP	22'11"
CLICK ARM	22'2"
ZAYO	21'1"
COMCAST ARM	21'1"
COMCAST	20'7"
WAVE ATTACH	21'7"

POLE NUMBER	TP29142
SECONDARY	25'2"
SECONDARY DRIP	24'6"
SL	21'5"
SL DRIP	21'5"
CLICK ARM	21'0"
COMCAST ARM	20'1"
ZAYO	20'1"
COMCAST	20'1"
WAVE ATTACH	20'1"

POLE NUMBER	TP29114
NEUTRAL PWR ARM	37'0"
PRIMARY PWR RISER	31'0"
SECONDARY	25'4"
SL	23'9"
CLICK 4 ARM	23'4"
SL DRIP	23'2"
STRAND 1	22'10"
ZAYO ARM	22'1"
COMCAST ARM	22'1"
STRAND 2	21'6"
WAVE ATTACH	22'10"

POLE NUMBER	TP26980
NEUTRAL ON POWER ARM	43'0"
PRIMARY POWER RISER	32'6"
SECONDARY	29'4"
SL	26'11"
SL DRIP	26'2"
CLICK 1 ARM	25'7"
CLICK 4	25'5"
F/O 016	24'4"
COMCAST	23'1"
TEL 4	22'2"
WAVE OVERLASH	24'10"

POLE NUMBER	TP29119
SECONDARY	26'11"
SECONDARY RACK (B/S)	23'9"
SECONDARY DRIP	23'6"
SL	22'3"
SL DRIP	22'2"
CLICK ARM	20'8"
ZAYO	20'1"
TEL	19'6"
COMCAST	18'6"
WAVE ATTACH	19'6"

PWR MR - LOWER CLICK ARM TO 20'1"
 MR - MOVE ZAYO TO 19'0"
 MR - MOVE TEL TO 18'6"
 MR - MOVE COMCAST TO 18'0"

PWR MR - BOND SL
 MR - MOVE COMCAST ARM TO 19'1"
 MR - MOVE ZAYO TO 19'1"
 PWR MR - BOND SL
 PWR MR - PLACE SL IN CONDUIT
 PWR MR - RAISE SECONDARY DRIP TO 26'8"
 MR - MOVE STRAND 1 TO 22'1"
 MR - MOVE ZAYO ARM TO 21'6"
 MR - MOVE COMCAST ARM TO 21'6"
 MR - MOVE STRAND 2 TO 21'10"

MATCH TO SHEET 5

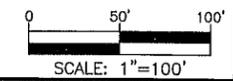
MATCH TO WAVE PROJECT 7487

MATCH TO WAVE PROJECT 7487

PLAN VIEW A



Know what's below.
Call before you dig.



3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT
Wave MGC TECHNICAL CONSULTING INC.				
WAVE ENGINEER: CHRISTOPHER MANTLE				
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY SHEET: 051 OF 22				

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ② STRAND ONLY ALONG EXISTING UTILITY POLES.

PWR MP - RAISE SECONDARY TO 23'9"
 PWR MP - RAISE SECONDARY DRIP TO 23'7"
 PWR MP - BOND SL

POLE NUMBER	TP28903
NEUTRAL	29'5"
SL	28'1"
SECONDARY	28'0"
SL DRIP	26'6"
F/O 1	23'8"
CLICK	22'9"
ZAYO	21'9"
COMCAST	20'7"
WAVE ATTACH	22'3"

POLE NUMBER	TP29134
SECONDARY RACK	26'2"
SECONDARY DRIP	26'2"
SL	24'8"
SL DRIP	23'11"
CLICK	21'6"
ZAYO	20'4"
COMCAST	19'6"
OHG PWR	17'1"
WAVE ATTACH	20'11"

POLE NUMBER	TP29135
SECONDARY RACK	24'9"
SECONDARY DRIP	24'9"
SL DRIP	24'3"
SL	24'1"
CLICK	21'4"
ZAYO	20'4"
COMCAST	19'4"
WAVE ATTACH	20'10"

PWR MR - LOWER CLICK TO 20'6"
 MR - MOVE ZAYO ARM TO 19'6"
 MR - MOVE COMCAST TO 9'0"

POLE NUMBER	TP29136
SECONDARY	25'2"
SECONDARY DRIP	24'2"
CLICK	23'11"
ZAYO ARM	21'4"
ZAYO	20'6"
COMCAST	19'6"
WAVE ATTACH	20'0"

POLE NUMBER	TP28114
SL 2 (B/S)	23'1"
SECONDARY	22'4"
SECONDARY DRIP	22'0"
SL DRIP-2	21'9"
SL	21'3"
SL DRIP	21'0"
SL SUPPORT 2	20'11"
CLICK	20'3"
ZAYO	19'2"
COMCAST	18'4"
WAVE ATTACH	19'9"

POLE NUMBER	TP29133
SECONDARY	29'0"
SECONDARY DRIP	28'2"
CLICK	21'8"
ZAYO	20'5"
COMCAST	19'6"
WAVE ATTACH	21'0"

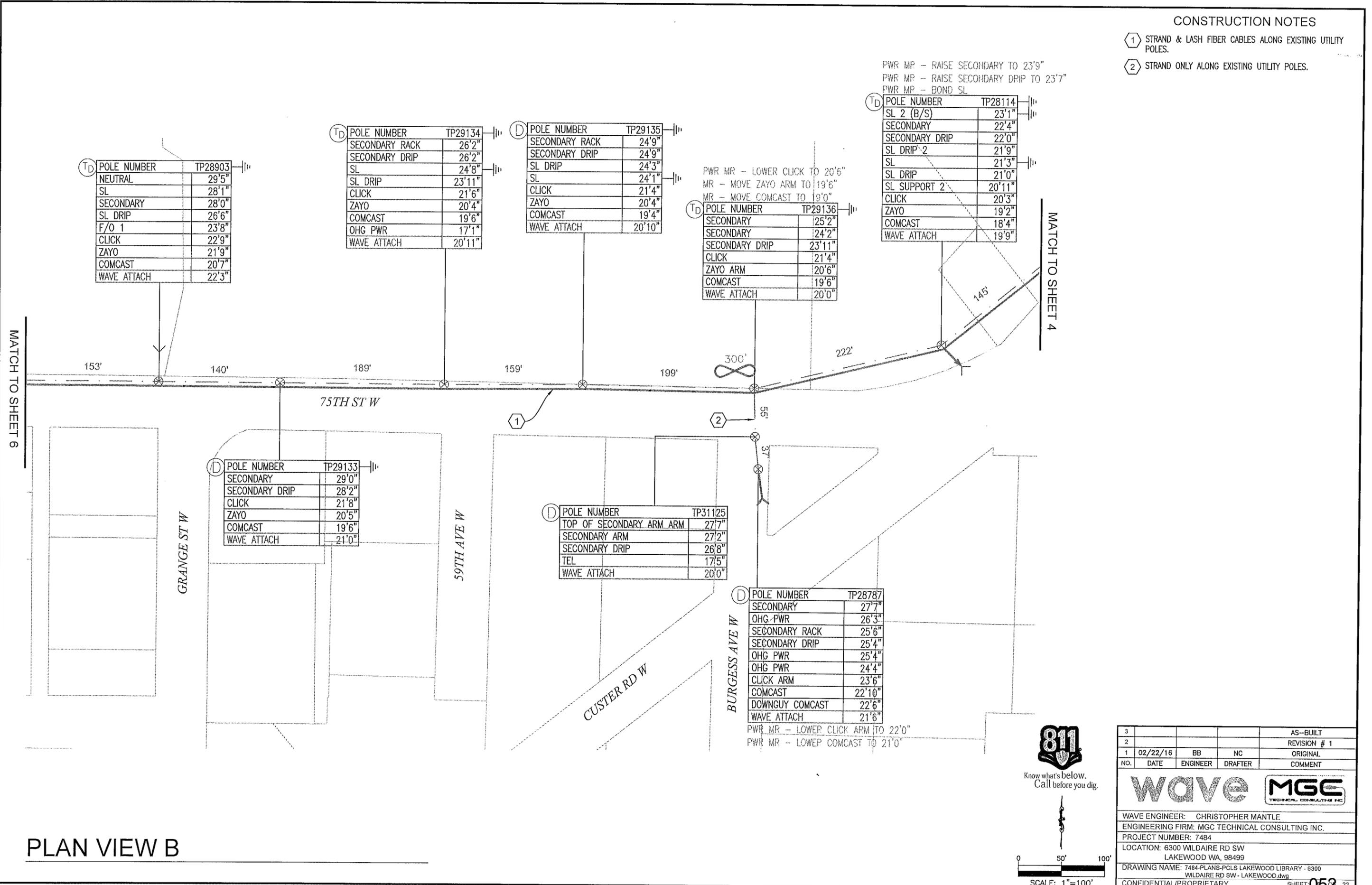
POLE NUMBER	TP31125
TOP OF SECONDARY ARM ARM	27'7"
SECONDARY ARM	27'2"
SECONDARY DRIP	26'8"
TEL	17'5"
WAVE ATTACH	20'0"

POLE NUMBER	TP28787
SECONDARY	27'7"
OHG-PWR	26'3"
SECONDARY RACK	25'6"
SECONDARY DRIP	25'4"
OHG PWR	25'4"
OHG PWR	24'4"
CLICK ARM	23'6"
COMCAST	22'10"
DOWNGUY COMCAST	22'6"
WAVE ATTACH	21'6"

PWR MR - LOWER CLICK ARM TO 22'0"
 PWR MR - LOWER COMCAST TO 21'0"

MATCH TO SHEET 6

MATCH TO SHEET 4

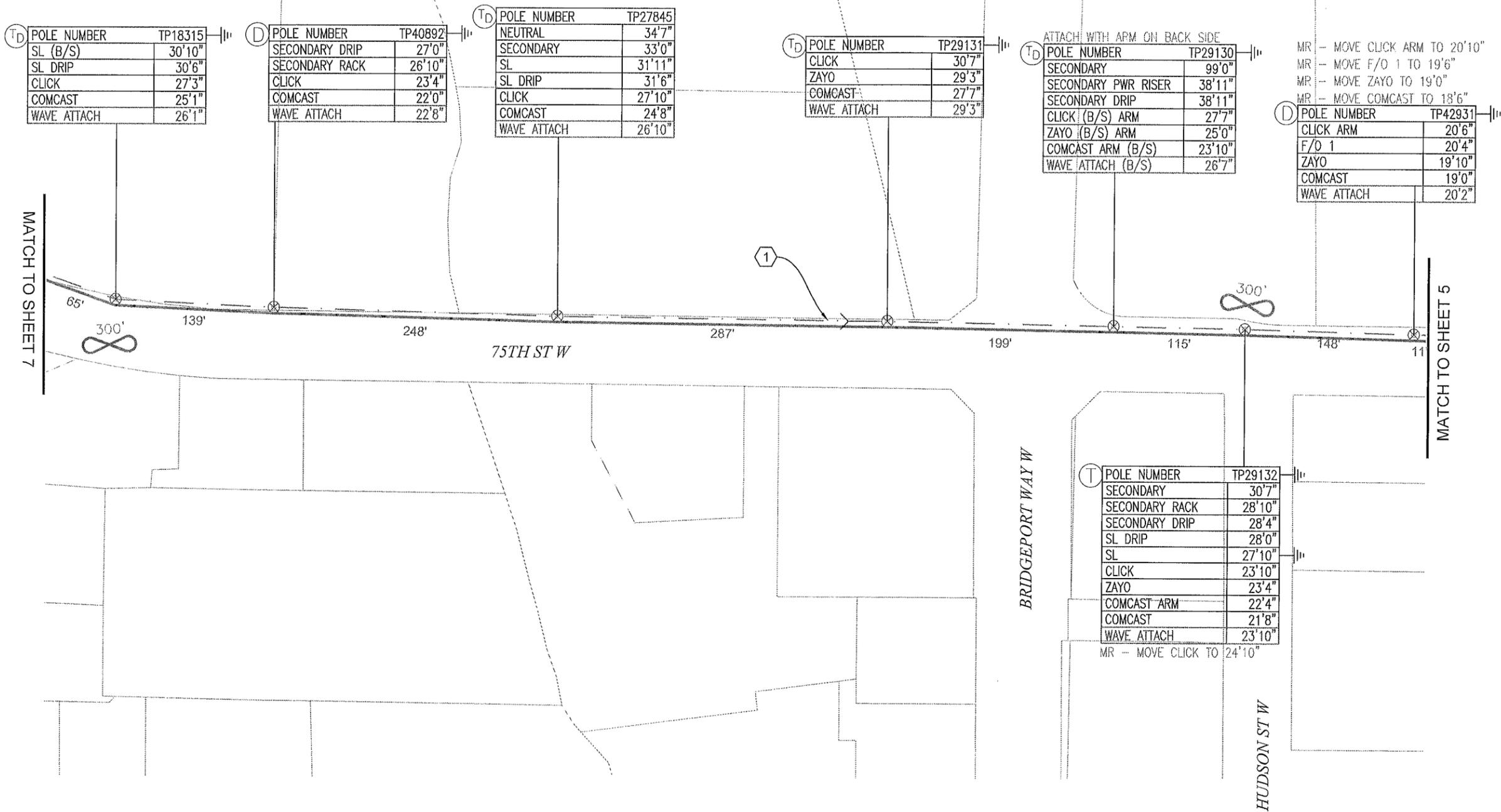


PLAN VIEW B

3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT
Wave		MGC		
WAVE ENGINEER: CHRISTOPHER MANTLE				
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY SHEET: 052 22				

CONSTRUCTION NOTES

- 1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



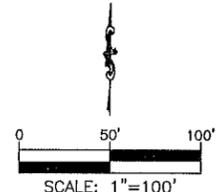
MATCH TO SHEET 7

MATCH TO SHEET 5

PLAN VIEW C



Know what's below.
Call before you dig.



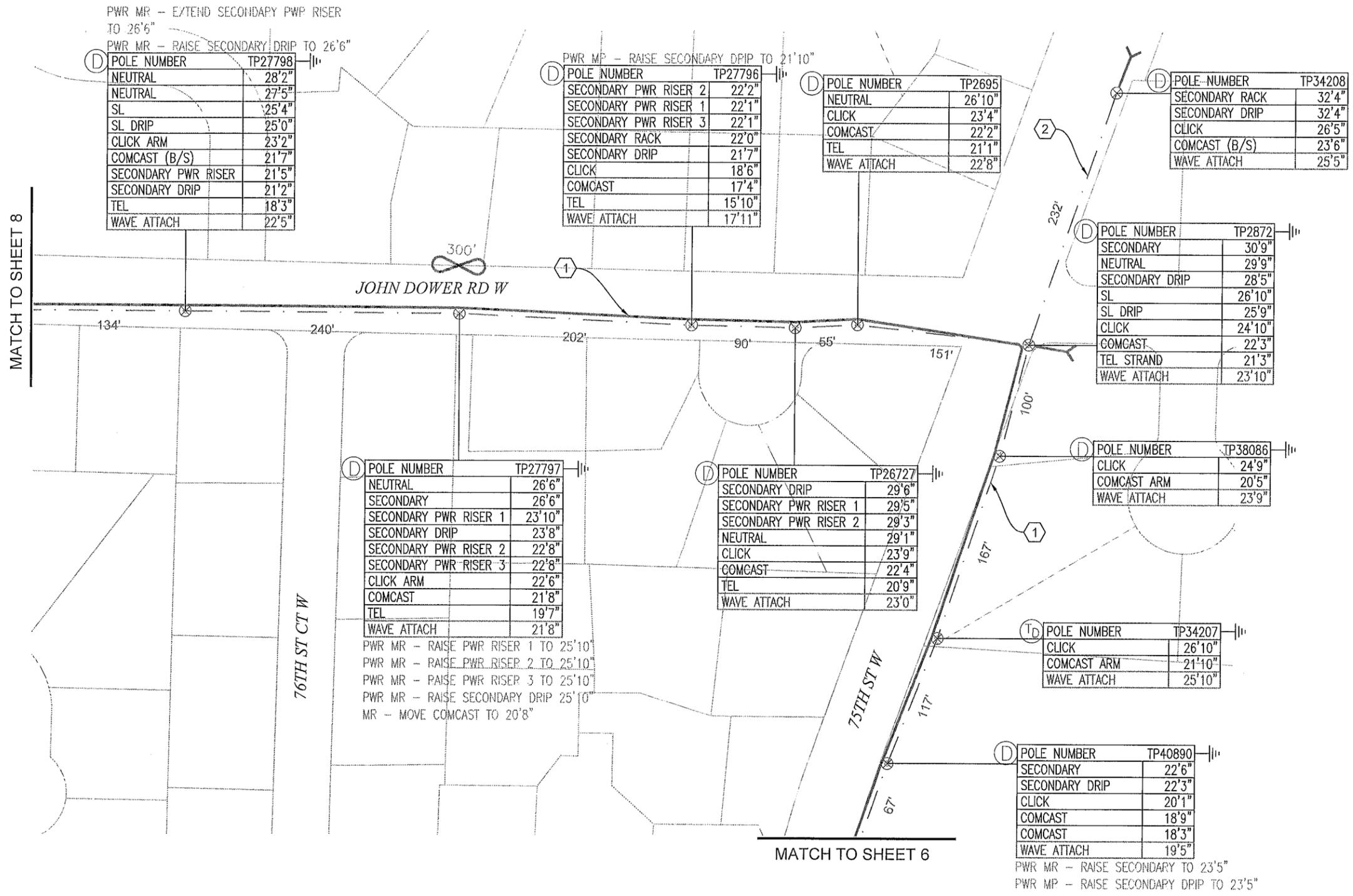
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

Wave **MGC**
TECHNICAL CONSULTING INC.

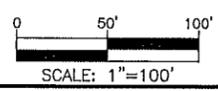
WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET 053 22

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ② STRAND ONLY ALONG EXISTING UTILITY POLES.



PLAN VIEW D



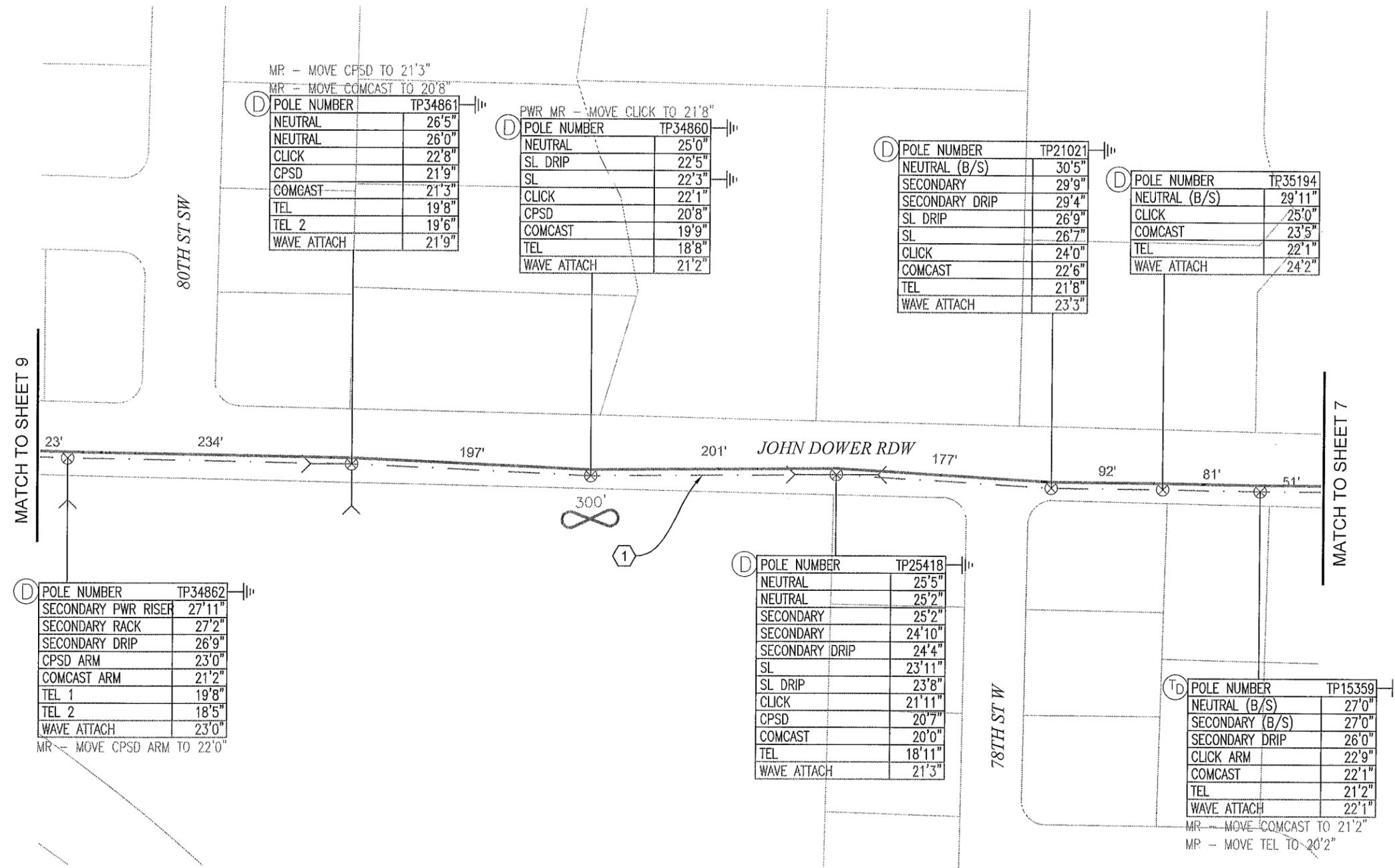
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

Wave **MGC**
TECHNICAL CONSULTING INC.

WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET: 054 22

CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



MR - MOVE CPSD ARM TO 22'0"

POLE NUMBER	TP34862
SECONDARY PWR RISER	27'11"
SECONDARY RACK	27'2"
SECONDARY DRIP	26'9"
CPSD ARM	23'0"
COMCAST ARM	21'2"
TEL 1	19'8"
TEL 2	18'5"
WAVE ATTACH	23'0"

MR - MOVE CPSD TO 21'3"
MR - MOVE COMCAST TO 20'8"

POLE NUMBER	TP34861
NEUTRAL	26'5"
NEUTRAL	26'0"
CLICK	22'8"
CPSD	21'9"
COMCAST	21'3"
TEL	19'8"
TEL 2	19'6"
WAVE ATTACH	21'9"

PWR MR - MOVE CLICK TO 21'8"

POLE NUMBER	TP34860
NEUTRAL	25'0"
SL DRIP	22'5"
SL	22'3"
CLICK	22'1"
CPSD	20'8"
COMCAST	19'9"
TEL	18'8"
WAVE ATTACH	21'2"

POLE NUMBER	TP21021
NEUTRAL (B/S)	30'5"
SECONDARY	29'9"
SECONDARY DRIP	29'4"
SL DRIP	26'9"
SL	26'7"
CLICK	24'0"
COMCAST	22'6"
TEL	21'8"
WAVE ATTACH	23'3"

POLE NUMBER	TP35194
NEUTRAL (B/S)	29'11"
CLICK	25'0"
COMCAST	23'5"
TEL	22'1"
WAVE ATTACH	24'2"

POLE NUMBER	TP25418
NEUTRAL	25'5"
NEUTRAL	25'2"
SECONDARY	25'2"
SECONDARY	24'10"
SECONDARY DRIP	24'4"
SL	23'11"
SL DRIP	23'8"
CLICK	21'11"
CPSD	20'7"
COMCAST	20'0"
TEL	18'11"
WAVE ATTACH	21'3"

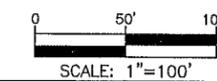
MR - MOVE COMCAST TO 21'2"
MR - MOVE TEL TO 20'2"

POLE NUMBER	TP15359
NEUTRAL (B/S)	27'0"
SECONDARY (B/S)	27'0"
SECONDARY DRIP	26'0"
CLICK ARM	22'9"
COMCAST	22'1"
TEL	21'2"
WAVE ATTACH	22'1"

PLAN VIEW E



Know what's below.
Call before you dig.



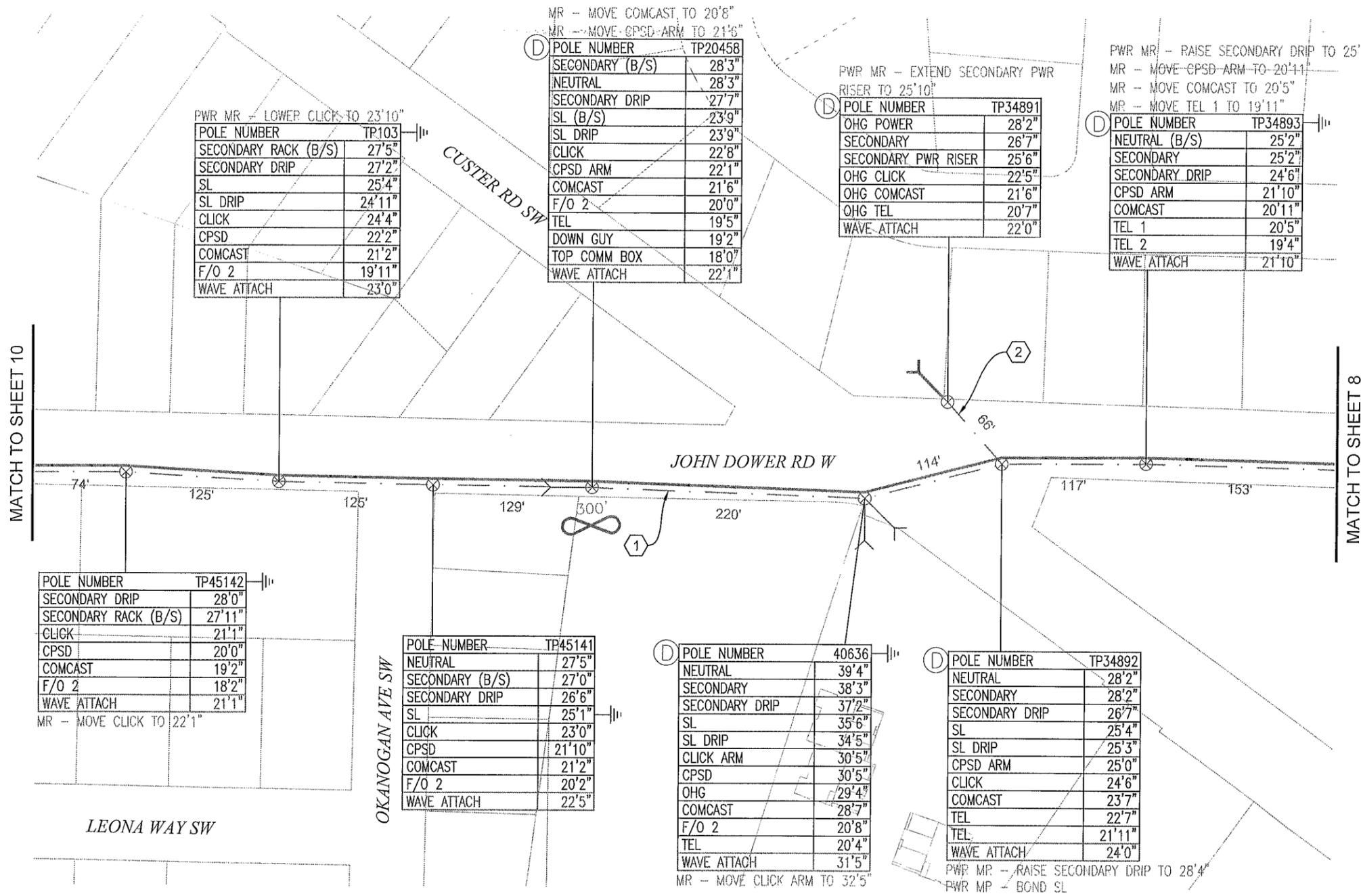
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

wave **MGC**
TECHNICAL CONSULTING INC.

WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET 055 22

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ② STRAND ONLY ALONG EXISTING UTILITY POLES.



PWR MR - LOWER CLICK TO 23'10"

POLE NUMBER	TP103
SECONDARY RACK (B/S)	27'5"
SECONDARY DRIP	27'2"
SL	25'4"
SL DRIP	24'11"
CLICK	24'4"
CPSD	22'2"
COMCAST	21'2"
F/O 2	19'11"
WAVE ATTACH	23'0"

MR - MOVE COMCAST TO 20'8"
MR - MOVE CPSD ARM TO 21'6"

POLE NUMBER	TP20458
SECONDARY (B/S)	28'3"
NEUTRAL	28'3"
SECONDARY DRIP	27'7"
SL (B/S)	23'9"
SL DRIP	23'9"
CLICK	22'8"
CPSD ARM	22'1"
COMCAST	21'6"
F/O 2	20'0"
TEL	19'5"
DOWN GUY	19'2"
TOP COMM BOX	18'0"
WAVE ATTACH	22'1"

PWR MR - EXTEND SECONDARY PWR RISER TO 25'10"

POLE NUMBER	TP34891
OHG POWER	28'2"
SECONDARY	26'7"
SECONDARY PWR RISER	25'6"
OHG CLICK	22'5"
OHG COMCAST	21'6"
OHG TEL	20'7"
WAVE ATTACH	22'0"

PWR MR - RAISE SECONDARY DRIP TO 25'2"
MR - MOVE CPSD ARM TO 20'11"
MR - MOVE COMCAST TO 20'5"
MR - MOVE TEL 1 TO 19'11"

POLE NUMBER	TP34893
NEUTRAL (B/S)	25'2"
SECONDARY	25'2"
SECONDARY DRIP	24'6"
CPSD ARM	21'10"
COMCAST	20'11"
TEL 1	20'5"
TEL 2	19'4"
WAVE ATTACH	21'10"

MR - MOVE CLICK TO 22'1"

POLE NUMBER	TP45142
SECONDARY DRIP	28'0"
SECONDARY RACK (B/S)	27'11"
CLICK	21'1"
CPSD	20'0"
COMCAST	19'2"
F/O 2	18'2"
WAVE ATTACH	21'1"

POLE NUMBER	TP45141
NEUTRAL	27'5"
SECONDARY (B/S)	27'0"
SECONDARY DRIP	26'6"
SL	25'1"
CLICK	23'0"
CPSD	21'10"
COMCAST	21'2"
F/O 2	20'2"
WAVE ATTACH	22'5"

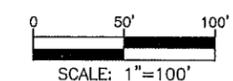
MR - MOVE CLICK ARM TO 32'5"

POLE NUMBER	40636
NEUTRAL	39'4"
SECONDARY	38'3"
SECONDARY DRIP	37'2"
SL	35'6"
SL DRIP	34'5"
CLICK ARM	30'5"
CPSD	30'5"
OHG	29'4"
COMCAST	28'7"
F/O 2	20'8"
TEL	20'4"
WAVE ATTACH	31'5"

PWR MR - RAISE SECONDARY DRIP TO 28'4"
PWR MP - BOND SL

POLE NUMBER	TP34892
NEUTRAL	28'2"
SECONDARY	28'2"
SECONDARY DRIP	26'7"
SL	25'4"
SL DRIP	25'3"
CPSD ARM	25'0"
CLICK	24'6"
COMCAST	23'7"
TEL	22'7"
TEL	21'11"
WAVE ATTACH	24'0"

PLAN VIEW F



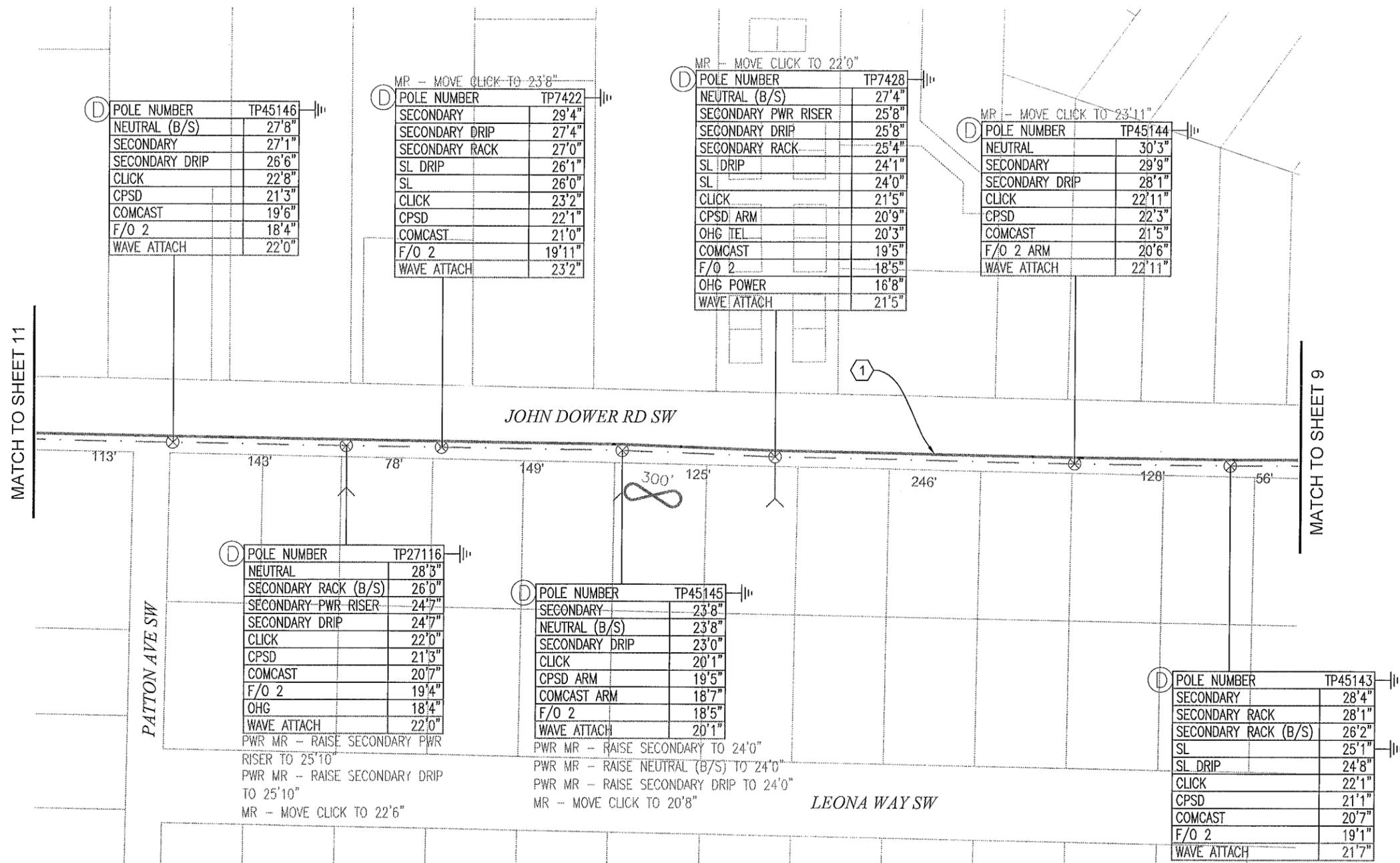
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

Wave **MGC**
TECHNICAL CONSULTING INC.

WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET: 056 22

CONSTRUCTION NOTES

- 1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



MR - MOVE CLICK TO 22'0"

POLE NUMBER	TP45146
NEUTRAL (B/S)	27'8"
SECONDARY	27'1"
SECONDARY DRIP	26'6"
CLICK	22'8"
CPSD	21'3"
COMCAST	19'6"
F/O 2	18'4"
WAVE ATTACH	22'0"

MR - MOVE CLICK TO 23'8"

POLE NUMBER	TP7422
SECONDARY	29'4"
SECONDARY DRIP	27'4"
SECONDARY RACK	27'0"
SL DRIP	26'1"
SL	26'0"
CLICK	23'2"
CPSD	22'1"
COMCAST	21'0"
F/O 2	19'11"
WAVE ATTACH	23'2"

MR - MOVE CLICK TO 22'0"

POLE NUMBER	TP7428
NEUTRAL (B/S)	27'4"
SECONDARY PWR RISER	25'8"
SECONDARY DRIP	25'8"
SECONDARY RACK	25'4"
SL DRIP	24'1"
SL	24'0"
CLICK	21'5"
CPSD ARM	20'9"
OHG TEL	20'3"
COMCAST	19'5"
F/O 2	18'5"
OHG POWER	16'8"
WAVE ATTACH	21'5"

MR - MOVE CLICK TO 23'11"

POLE NUMBER	TP45144
NEUTRAL	30'3"
SECONDARY	29'9"
SECONDARY DRIP	28'1"
CLICK	22'11"
CPSD	22'3"
COMCAST	21'5"
F/O 2 ARM	20'6"
WAVE ATTACH	22'11"

POLE NUMBER	TP27116
NEUTRAL	28'3"
SECONDARY RACK (B/S)	26'0"
SECONDARY PWR RISER	24'7"
SECONDARY DRIP	24'7"
CLICK	22'0"
CPSD	21'3"
COMCAST	20'7"
F/O 2	19'4"
OHG	18'4"
WAVE ATTACH	22'0"

POLE NUMBER	TP45145
SECONDARY	23'8"
NEUTRAL (B/S)	23'8"
SECONDARY DRIP	23'0"
CLICK	20'1"
CPSD ARM	19'5"
COMCAST ARM	18'7"
F/O 2	18'5"
WAVE ATTACH	20'1"

POLE NUMBER	TP45143
SECONDARY	28'4"
SECONDARY RACK	28'1"
SECONDARY RACK (B/S)	26'2"
SL	25'1"
SL DRIP	24'8"
CLICK	22'1"
CPSD	21'1"
COMCAST	20'7"
F/O 2	19'1"
WAVE ATTACH	21'7"

PWR MR - RAISE SECONDARY PWR RISER TO 25'10"
 PWR MR - RAISE SECONDARY DRIP TO 25'10"
 MR - MOVE CLICK TO 22'6"

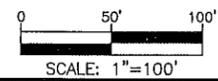
PWR MR - RAISE SECONDARY TO 24'0"
 PWR MR - RAISE NEUTRAL (B/S) TO 24'0"
 PWR MR - RAISE SECONDARY DRIP TO 24'0"
 MR - MOVE CLICK TO 20'8"

LEONA WAY SW

MATCH TO SHEET 11

MATCH TO SHEET 9

PLAN VIEW G



3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

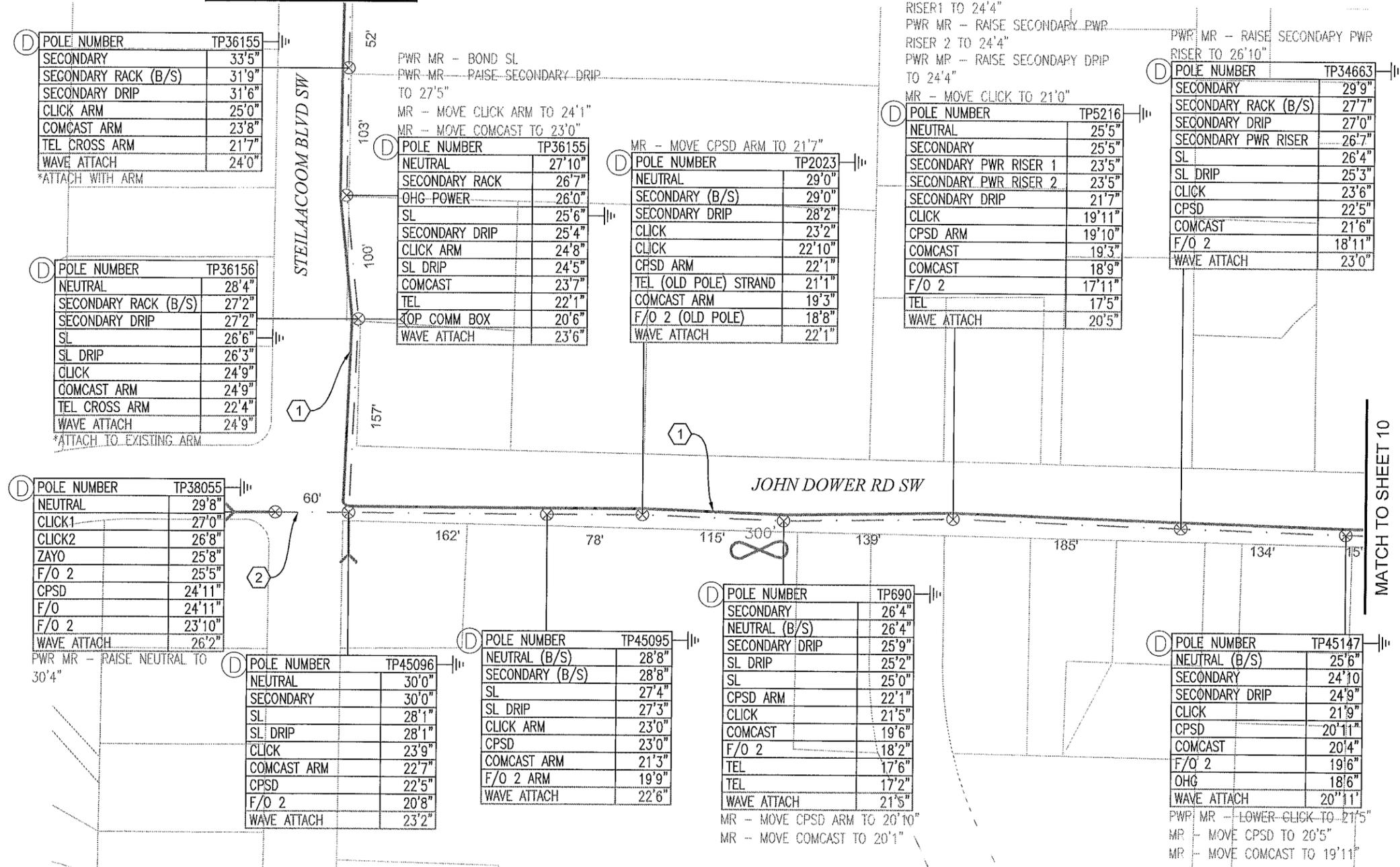
Wave **MGC**
 TECHNICAL CONSULTING INC.

WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 057 OF 22

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ② STRAND ONLY ALONG EXISTING UTILITY POLES.

MATCH TO SHEET 12



①

POLE NUMBER	TP36155
SECONDARY	33'5"
SECONDARY RACK (B/S)	31'9"
SECONDARY DRIP	31'6"
CLICK ARM	25'0"
COMCAST ARM	23'8"
TEL CROSS ARM	21'7"
WAVE ATTACH	24'0"

*ATTACH WITH ARM

①

POLE NUMBER	TP36156
NEUTRAL	28'4"
SECONDARY RACK (B/S)	27'2"
SECONDARY DRIP	27'2"
SL	26'6"
SL DRIP	26'3"
CLICK	24'9"
COMCAST ARM	24'9"
TEL CROSS ARM	22'4"
WAVE ATTACH	24'9"

*ATTACH TO EXISTING ARM

①

POLE NUMBER	TP38055
NEUTRAL	29'8"
CLICK1	27'0"
CLICK2	26'8"
ZAYO	25'8"
F/O 2	25'5"
CPSD	24'11"
F/O	24'11"
F/O 2	23'10"
WAVE ATTACH	26'2"

①

POLE NUMBER	TP45096
NEUTRAL	30'0"
SECONDARY	30'0"
SL	28'1"
SL DRIP	28'1"
CLICK	23'9"
COMCAST ARM	22'7"
CPSD	22'5"
F/O 2	20'8"
WAVE ATTACH	23'2"

①

POLE NUMBER	TP36155
NEUTRAL	27'10"
SECONDARY RACK	26'7"
OHG POWER	26'0"
SL	25'6"
SECONDARY DRIP	25'4"
CLICK ARM	24'8"
SL DRIP	24'5"
COMCAST	23'7"
TEL	22'1"
TOP COMM BOX	20'6"
WAVE ATTACH	23'6"

①

POLE NUMBER	TP2023
NEUTRAL	29'0"
SECONDARY (B/S)	29'0"
SECONDARY DRIP	28'2"
CLICK	23'2"
CLICK	22'10"
CPSD ARM	22'1"
TEL (OLD POLE) STRAND	21'1"
COMCAST ARM	19'3"
F/O 2 (OLD POLE)	18'8"
WAVE ATTACH	22'1"

PWR MR -- RAISE SECONDARY PWR RISER1 TO 24'4"
 PWR MR -- RAISE SECONDARY PWR RISER 2 TO 24'4"
 PWR MR -- RAISE SECONDARY DRIP TO 24'4"
 MR -- MOVE CLICK TO 21'0"

①

POLE NUMBER	TP5216
NEUTRAL	25'5"
SECONDARY	25'5"
SECONDARY PWR RISER 1	23'5"
SECONDARY PWR RISER 2	23'5"
SECONDARY DRIP	21'7"
CLICK	19'11"
CPSD ARM	19'10"
CLICK	19'3"
COMCAST	18'9"
F/O 2	17'11"
TEL	17'5"
WAVE ATTACH	20'5"

①

POLE NUMBER	TP34663
SECONDARY	29'9"
SECONDARY RACK (B/S)	27'7"
SECONDARY DRIP	27'0"
SECONDARY PWR RISER	26'7"
SL	26'4"
SL DRIP	25'3"
CLICK	23'6"
CPSD	22'5"
COMCAST	21'6"
F/O 2	18'11"
WAVE ATTACH	23'0"

①

POLE NUMBER	TP690
SECONDARY	26'4"
NEUTRAL (B/S)	26'4"
SECONDARY DRIP	25'9"
SL DRIP	25'2"
SL	25'0"
CPSD ARM	22'1"
CLICK	21'5"
COMCAST	19'6"
F/O 2	18'2"
TEL	17'6"
TEL	17'2"
WAVE ATTACH	21'5"

①

POLE NUMBER	TP45095
NEUTRAL (B/S)	28'8"
SECONDARY (B/S)	28'8"
SL	27'4"
SL DRIP	27'3"
CLICK ARM	23'0"
CPSD	23'0"
COMCAST ARM	21'3"
F/O 2 ARM	19'9"
WAVE ATTACH	22'6"

①

POLE NUMBER	TP45147
NEUTRAL (B/S)	25'6"
SECONDARY	24'10"
SECONDARY DRIP	24'9"
CLICK	21'9"
CPSD	20'11"
COMCAST	20'4"
F/O 2	19'6"
OHG	18'6"
WAVE ATTACH	20'11"

MR -- MOVE CPSD ARM TO 20'10"
 MR -- MOVE COMCAST TO 20'1"

PWR MR -- LOWER CLICK TO 21'5"
 MR -- MOVE CPSD TO 20'5"
 MR -- MOVE COMCAST TO 19'11"



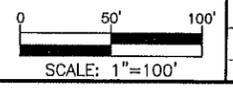
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3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 058 22

PLAN VIEW H



CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.

PWR MR - LOWER CLICK ARM TO 22'9"
 PWR MP - LOWER ZAYO TO 21'9"
 MR - MOVE COMCAST & CPSD ARM TO 21'1"
 MR - MOVE TRAFFIC SIGNAL F/O TO 20'7"

MP - MOVE CLICK TO 22'10"

POLE NUMBER	TP5053
NEUTRAL (B/S)	29'6"
SECONDARY RACK (B/S)	26'10"
SECONDARY DRIP	26'7"
CLICK	21'10"
COMCAST	20'10"
TEL	19'3"
WAVE ATTACH	21'10"

POLE NUMBER	TP45006
NEUTRAL (B/S)	31'0"
SECONDARY DRIP	30'5"
SL	27'3"
SL DRIP	27'2"
CLICK (B/S)	26'9"
COMCAST (B/S)	25'7"
TEL (OLD POLE)	22'11"
TEL 2 (OLD POLE)	22'1"
TEL 3 (OLD POLE)	20'10"
WAVE ATTACH	26'2"

MR - MOVE CLICK TO 22'10"

POLE NUMBER	TP45005
SECONDARY RACK (B/S)	26'2"
SECONDARY DRIP	26'2"
CLICK	22'2"
COMCAST	21'3"
TEL	20'3"
WAVE ATTACH	22'2"

POLE NUMBER	TP38841
NEUTRAL	26'4"
SECONDARY PWR RISER	26'0"
SECONDARY (B/S)	26'0"
SECONDARY DRIP	25'2"
CLICK ARM	24'1"
ZAYO	23'6"
COMCAST ARM	22'9"
CPSD ARM	22'9"
TRAFFIC SIGNAL F/O	21'9"
TEL CROSS ARM	20'1"
F/O TRAFFIC SIGNAL	19'7"
WAVE ATTACH	22'3"

POLE NUMBER	TP36152
NEUTRAL ARM	29'0"
CLICK	24'11"
COMCAST ARM	23'2"
TEL CROSS ARM	21'3"
DOWN GUY	20'1"
WAVE ATTACH	23'8"

MATCH TO SHEET 13

POLE NUMBER	TP45011
SECONDARY RACK (B/S)	26'6"
SECONDARY DRIP	26'2"
SL	24'11"
SL DRIP	24'9"
CLICK	21'11"
COMCAST	20'11"
TEL STRAND	19'10"
WAVE ATTACH	21'11"

MR - MOVE CLICK TO 22'11"

POLE NUMBER	TP45009
NEUTRAL (B/S)	30'11"
SL	29'11"
SL DRIP	29'11"
CLICK (B/S)	23'9"
COMCAST (B/S)	22'6"
TEL (B/S)	20'10"
WAVE ATTACH (B/S)	23'1"

POLE NUMBER	TP45004
SECONDARY	28'3"
SECONDARY DRIP	28'3"
NEUTRAL	27'11"
CLICK	22'4"
COMCAST	21'1"
DOWN GUY	19'11"
TEL	19'10"
TEL	19'2"
TOP COMM BOX	18'11"
WAVE ATTACH	21'7"

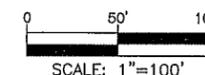
POLE NUMBER	TP36153
NEUTRAL (B/S)	29'5"
SECONDARY RACK	26'7"
SECONDARY DRIP	26'1"
CLICK ARM	24'0"
COMCAST ARM	22'5"
TEL CROSS ARM	21'5"
TOP COMM BOX	20'2"
WAVE ATTACH	23'0"

PWR MR - RAISE SECONDARY RACK TO 27'6"
 PWR MR - RAISE SECONDARY DRIP TO 27'4"
 ATTACH WITH ARM

PLAN VIEW I



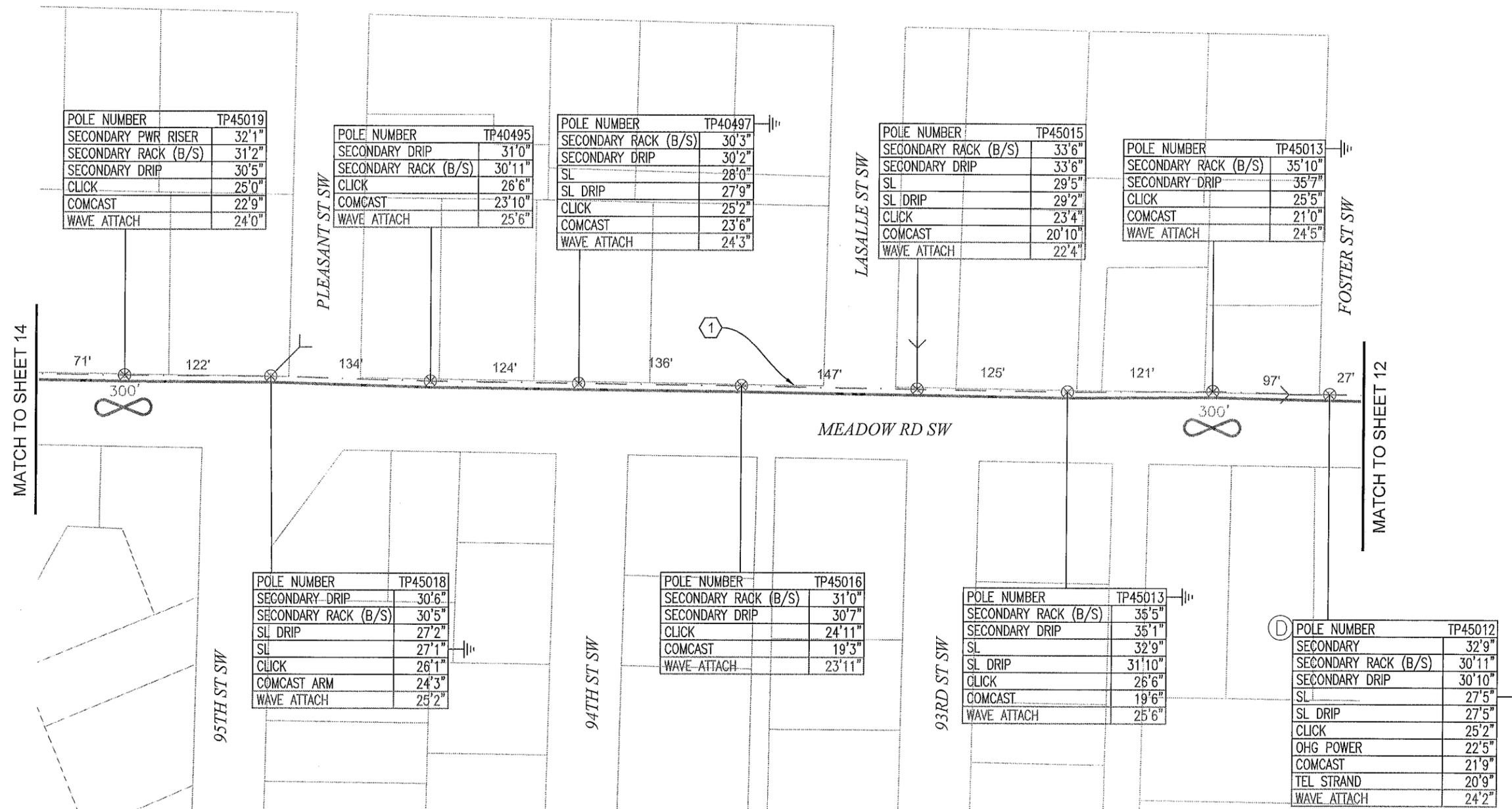
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2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT
wave		MGC		TECHNICAL CONSULTING INC.
WAVE ENGINEER: CHRISTOPHER MANTLE				
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY				SHEET 059 22

CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



MATCH TO SHEET 14

MATCH TO SHEET 12

MEADOW RD SW

PLEASANT ST SW

LASALLE ST SW

FOSTER ST SW

95TH ST SW

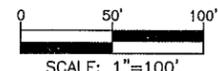
94TH ST SW

93RD ST SW

PLAN VIEW J



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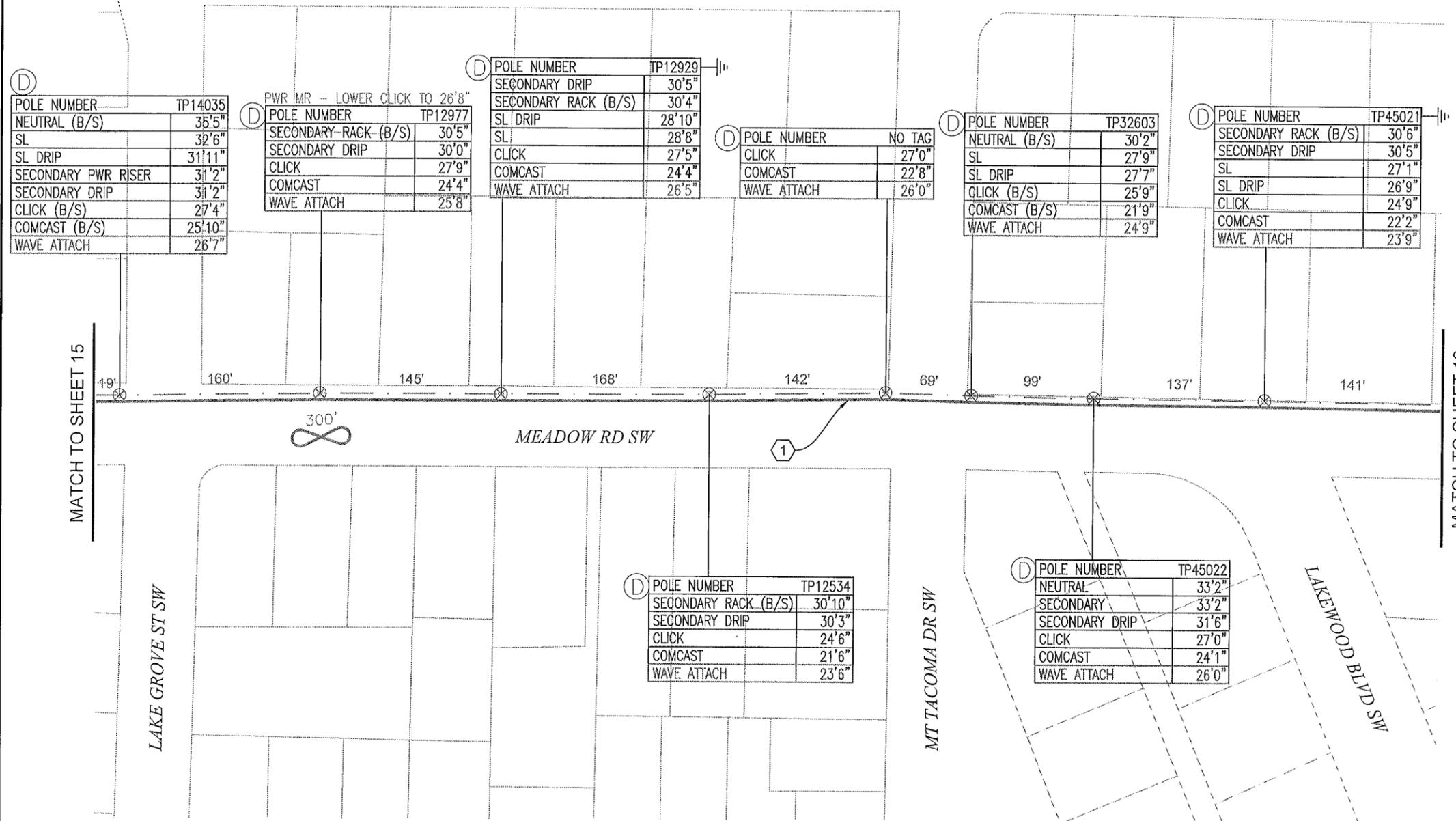
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 060 OF 22

CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



POLE NUMBER	TP14035
NEUTRAL (B/S)	35'5"
SL	32'6"
SL DRIP	31'11"
SECONDARY PWR RISER	31'2"
SECONDARY DRIP	31'2"
CLICK (B/S)	27'4"
COMCAST (B/S)	25'10"
WAVE ATTACH	26'7"

PWR IMR - LOWER CLICK TO 26'8"

POLE NUMBER	TP12977
SECONDARY RACK (B/S)	30'5"
SECONDARY DRIP	30'0"
CLICK	27'9"
COMCAST	24'4"
WAVE ATTACH	25'8"

POLE NUMBER	TP12929
SECONDARY DRIP	30'5"
SECONDARY RACK (B/S)	30'4"
SL DRIP	28'10"
SL	28'8"
CLICK	27'5"
COMCAST	24'4"
WAVE ATTACH	26'5"

POLE NUMBER	NO TAG
CLICK	27'0"
COMCAST	22'8"
WAVE ATTACH	26'0"

POLE NUMBER	TP32603
NEUTRAL (B/S)	30'2"
SL	27'9"
SL DRIP	27'7"
CLICK (B/S)	25'9"
COMCAST (B/S)	21'9"
WAVE ATTACH	24'9"

POLE NUMBER	TP45021
SECONDARY RACK (B/S)	30'6"
SECONDARY DRIP	30'5"
SL	27'1"
SL DRIP	26'9"
CLICK	24'9"
COMCAST	22'2"
WAVE ATTACH	23'9"

POLE NUMBER	TP12534
SECONDARY RACK (B/S)	30'10"
SECONDARY DRIP	30'3"
CLICK	24'6"
COMCAST	21'6"
WAVE ATTACH	23'6"

POLE NUMBER	TP45022
NEUTRAL	33'2"
SECONDARY	33'2"
SECONDARY DRIP	31'6"
CLICK	27'0"
COMCAST	24'1"
WAVE ATTACH	26'0"

PLAN VIEW K



Know what's below.
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SCALE: 1"=100'

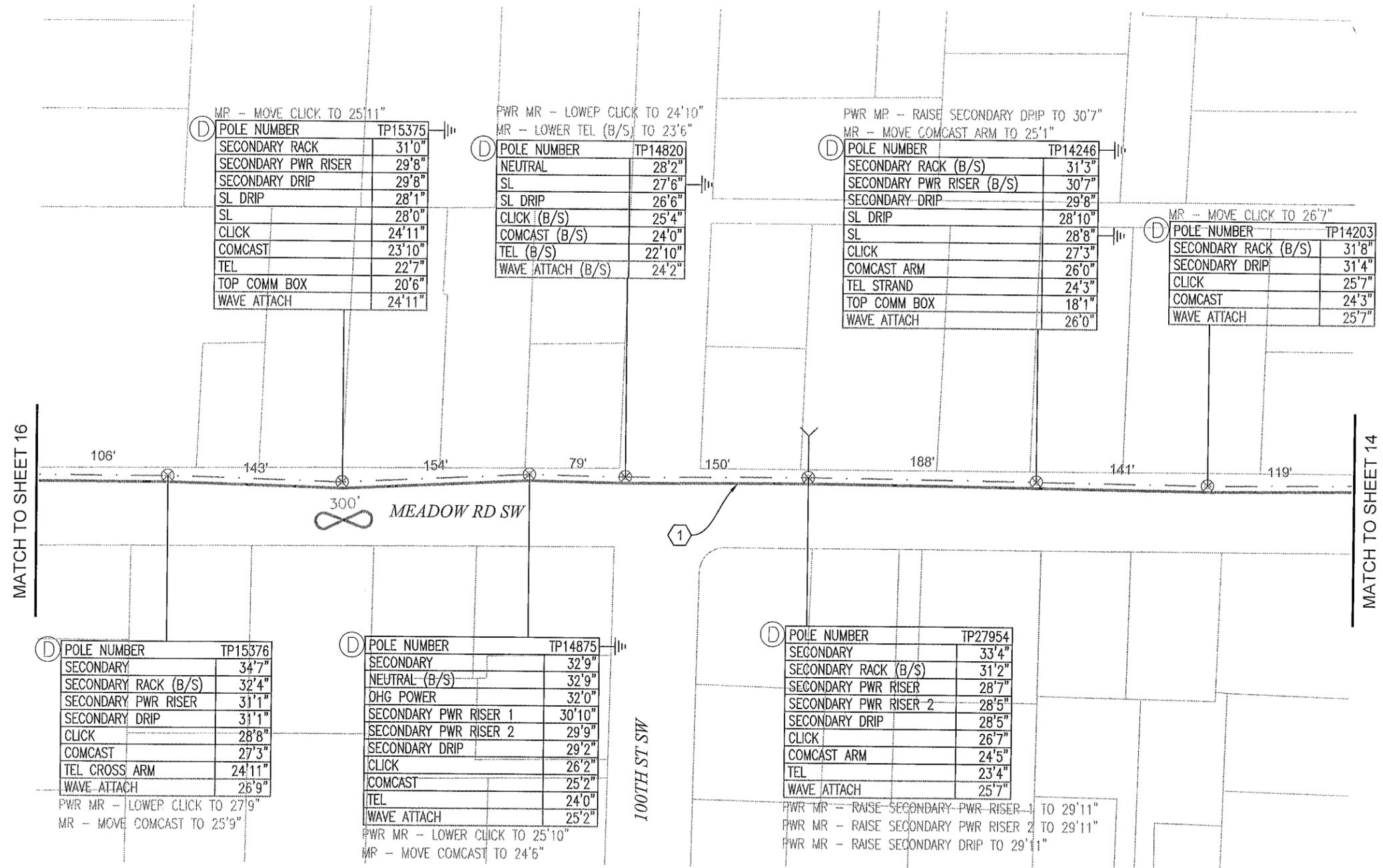
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-FCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 061 OF 22

CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.



MP - MOVE CLICK TO 25'11"

POLE NUMBER	TP15375
SECONDARY RACK	31'0"
SECONDARY PWR RISER	29'8"
SECONDARY DRIP	29'8"
SL DRIP	28'1"
SL	28'0"
CLICK	24'11"
COMCAST	23'10"
TEL	22'7"
TOP COMM BOX	20'6"
WAVE ATTACH	24'11"

PWR MR - LOWER CLICK TO 24'10"
MR - LOWER TEL (B/S) TO 23'6"

POLE NUMBER	TP14820
NEUTRAL	28'2"
SL	27'6"
SL DRIP	26'6"
CLICK (B/S)	25'4"
COMCAST (B/S)	24'0"
TEL (B/S)	22'10"
WAVE ATTACH (B/S)	24'2"

PWR MR - RAISE SECONDARY DRIP TO 30'7"
MR - MOVE COMCAST ARM TO 25'1"

POLE NUMBER	TP14246
SECONDARY RACK (B/S)	31'3"
SECONDARY PWR RISER (B/S)	30'7"
SECONDARY DRIP	29'8"
SL DRIP	28'10"
SL	28'8"
CLICK	27'3"
COMCAST ARM	26'0"
TEL STRAND	24'3"
TOP COMM BOX	18'1"
WAVE ATTACH	26'0"

MR - MOVE CLICK TO 26'7"

POLE NUMBER	TP14203
SECONDARY RACK (B/S)	31'8"
SECONDARY DRIP	31'4"
CLICK	25'7"
COMCAST	24'3"
WAVE ATTACH	25'7"

POLE NUMBER	TP15376
SECONDARY	34'7"
SECONDARY RACK (B/S)	32'4"
SECONDARY PWR RISER	31'1"
SECONDARY DRIP	31'1"
CLICK	28'8"
COMCAST	27'3"
TEL CROSS ARM	24'11"
WAVE ATTACH	26'9"

PWR MR - LOWER CLICK TO 27'9"
MR - MOVE COMCAST TO 25'9"

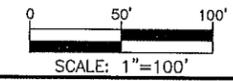
POLE NUMBER	TP14875
SECONDARY	32'9"
NEUTRAL (B/S)	32'9"
OHG POWER	32'0"
SECONDARY PWR RISER 1	30'10"
SECONDARY PWR RISER 2	29'9"
SECONDARY DRIP	29'2"
CLICK	26'2"
COMCAST	25'2"
TEL	24'0"
WAVE ATTACH	25'2"

PWR MR - LOWER CLICK TO 25'10"
MR - MOVE COMCAST TO 24'6"

POLE NUMBER	TP27954
SECONDARY	33'4"
SECONDARY RACK (B/S)	31'2"
SECONDARY PWR RISER	28'7"
SECONDARY PWR RISER 2	28'5"
SECONDARY DRIP	28'5"
CLICK	26'7"
COMCAST ARM	24'5"
TEL	23'4"
WAVE ATTACH	25'7"

PWR MR - RAISE SECONDARY PWR RISER 1 TO 29'11"
PWR MR - RAISE SECONDARY PWR RISER 2 TO 29'11"
PWR MR - RAISE SECONDARY DRIP TO 29'11"

PLAN VIEW L



3				AS-BUILT
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NO.	DATE	ENGINEER	DRAFTER	COMMENT
wave MGC TECHNICAL CONSULTING INC.				
WAVE ENGINEER: CHRISTOPHER MANTLE				
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.				
PROJECT NUMBER: 7484				
LOCATION: 6300 WILDAIRE RD SW LAKEWOOD WA, 98499				
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300 WILDAIRE RD SW - LAKEWOOD.dwg				
CONFIDENTIAL/PROPRIETARY SHEET: 062 22				

CONSTRUCTION NOTES

1 STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.

MATCH TO SHEET 15

POLE NUMBER	TP15521
NEUTRAL	30'6"
SL	27'1"
SL DRIP	26'11"
SL (B/S)	25'10"
OHG POWER	25'0"
CLICK	24'6"
COMCAST (B/S)	24'0"
TEL CROSS ARM	23'6"
WAVE ATTACH	24'6"

MR - MOVE OHG POWER TO 25'6"
MR - MOVE CLICK TO 25'0"

MR - MOVE COMCAST TO 23'4"

POLE NUMBER	TP15847
SECONDARY	33'6"
OHG POWER	32'8"
SECONDARY RACK	31'4"
SECONDARY DRIP	31'1"
CLICK	25'5"
COMCAST	24'4"
TEL	22'6"
WAVE ATTACH	24'4"

POLE NUMBER	TP32469
NEUTRAL	27'3"
SL	25'5"
SL DRIP	25'4"
CLICK	24'2"
CLICK	23'9"
COMCAST	22'9"
COMCAST	22'6"
WAVE ATTACH	23'6"

POLE NUMBER	TP15920
NEUTRAL	33'2"
SECONDARY RACK	31'0"
OHG POWER	30'4"
SECONDARY DRIP	30'0"
CLICK	23'6"
COMCAST	21'7"
WAVE ATTACH	22'6"

POLE NUMBER	TP15926
NEUTRAL	29'5"
SECONDARY RACK	27'2"
SECONDARY DRIP	27'1"
CLICK	22'11"
CLICK	22'5"
COMCAST	21'7"
WAVE ATTACH	21'7"

MR - MOVE COMCAST TO 21'1"

POLE NUMBER	TP32954
SECONDARY	27'9"
NEUTRAL	27'9"
SECONDARY DRIP	26'11"
SL	25'6"
SL DRIP	25'5"
CLICK ARM	25'0"
COMCAST	24'6"
WAVE ATTACH	23'5"

PWR MR - RAISE SECONDARY DRIP TO 27'9"
PWR MR - LOWER CLICK TO 24'5"
PWR MR - LOWER COMCAST TO 22'5"
PWR MR - BOND SL
PWR MR - PLACE SL DRIP IN CONDUIT

POLE NUMBER	TP33296
SECONDARY	28'6"
SECONDARY RACK (B/S)	26'4"
SECONDARY DRIP	26'4"
CLICK	23'0"
COMCAST	19'7"
WAVE ATTACH	22'0"

PWR MR - LOWER CLICK ARM TO 22'6"
PWR MR - LOWER COMCAST TO 21'6"

POLE NUMBER	TP34516
SECONDARY RACK (B/S)	26'2"
SECONDARY DRIP	25'10"
SL	24'7"
SL DRIP	24'4"
CLICK ARM	24'2"
COMCAST	23'5"
WAVE ATTACH	22'0"

POLE NUMBER	TP29439
NEUTRAL	26'7"
CLICK	24'0"
COMCAST	22'8"
TEL	21'8"
WAVE ATTACH	22'8"

PWR MR - LOWER CLICK TO 23'2"
MR - MOVE COMCAST TO 22'2"

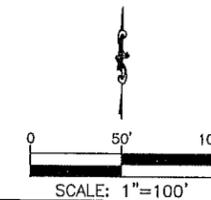
POLE NUMBER	TP33996
SECONDARY	30'0"
SECONDARY RACK (B/S)	28'2"
SECONDARY DRIP	27'0"
CLICK (B/S)	23'1"
COMCAST (B/S)	21'9"
WAVE ATTACH	22'6"

MATCH TO SHEET 17

PLAN VIEW M



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NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET: 063 22

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ② STRAND ONLY ALONG EXISTING UTILITY POLES.

POLE NUMBER	TP28297
SECONDARY RACK	26'3"
SECONDARY PWR RISER	23'8"
SECONDARY DRIP	23'8"
CLICK	21'11"
COMCAST	20'9"
WAVE ATTACH	21'11"

PWR MR - EXTEND SECONDARY PWR RISER TO 26'3"
 PWR MR - RAISE SECONDARY DRIP TO 26'3"
 MR - MOVE CLICK TO 22'11"

POLE NUMBER	TP28294
SECONDARY RACK	29'3"
SECONDARY DRIP	29'2"
CLICK	25'9"
COMCAST	24'7"
WAVE ATTACH	24'7"

MR - MOVE COMCAST TO 23'7"

POLE NUMBER	TP28293
NEUTRAL	28'9"
CLICK	20'3"
COMCAST	18'9"
WAVE ATTACH	19'3"

POLE NUMBER	TP45341
NEUTRAL	27'5"
SECONDARY RACK	26'2"
SECONDARY PWR RISER 1	24'10"
SECONDARY PWR RISER 3	24'4"
SECONDARY PWR RISER 2	24'2"
SECONDARY DRIP	24'2"
SL	23'11"
CLICK	20'10"
COMCAST	19'10"
WAVE ATTACH	20'4"

POLE NUMBER	TP28298
SECONDARY	29'9"
SECONDARY RACK	27'7"
SECONDARY PWR RISER 1	26'11"
SL	26'0"
SL DRIP	26'0"
SECONDARY PWR RISER 2	25'8"
SECONDARY PWR RISER 3	25'3"
CLICK	21'11"
COMCAST	20'4"
TOP COMM BOX	18'0"
WAVE ATTACH	21'5"

POLE NUMBER	TP28299
NEUTRAL	30'2"
SECONDARY DRIP	29'1"
CLICK	25'9"
COMCAST	23'1"
WAVE ATTACH	24'9"

POLE NUMBER	TP28304
SECONDARY	27'1"
NEUTRAL	26'7"
SECONDARY DRIP	24'0"
CLICK	22'11"
COMCAST	21'5"
WAVE ATTACH	21'11"

POLE NUMBER	TP35165
SECONDARY	29'1"
SECONDARY DRIP	27'2"
SECONDARY RACK	26'11"
CLICK (B/S)	22'11"
COMCAST (B/S)	21'5"
WAVE ATTACH (B/S)	22'5"

POLE NUMBER	TP28300
SECONDARY	31'1"
SL	29'7"
SECONDARY RACK	28'11"
SL DRIP	28'10"
SECONDARY PWR RISER	27'3"
SECONDARY DRIP	26'10"
COMMUNICATIONS ARM	22'4"
WAVE ATTACH	22'4"

*WAVE TO ATTACH TO COMMUNICATIONS ARM

POLE NUMBER	TP40787
NEUTRAL (B/S)	28'7"
SECONDARY RACK	26'1"
SECONDARY PWR RISER	24'8"
SECONDARY DRIP	24'8"
CLICK	21'11"
COMCAST	20'9"
WAVE ATTACH	20'9"

PWR MR - LOWER CLICK TO 21'3"
 MR - MOVE COMCAST TO 20'3"

POLE NUMBER	TP31011
NEUTRAL	24'4"
SECONDARY DRIP	21'8"
SECONDARY RACK	21'6"
CLICK	19'3"
COMCAST	17'11"
WAVE ATTACH	19'3"

PWR MR - RAISE SECONDARY RACK TO 24'0"
 PWR MR - RAISE SECONDARY DRIP TO 24'0"
 MR - MOVE CLICK TO 20'3"

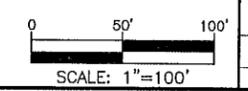
POLE NUMBER	TP28306
SECONDARY	23'6"
OHG CLICK	19'5"
OHG COMCAST	18'11"
SECONDARY PWR RISER 1	18'2"
SECONDARY PWR RISER 2	18'0"
SECONDARY DRIP	18'0"
WAVE ATTACH	19'5"

PWR MR - EXTEND SECONDARY PWR RISER 1, 2, AND 3 TO 23'6"
 PWR MR - RAISE SECONDARY DRIP TO 23'6"
 MR - MOVE OHG TO 19'11"

MATCH TO SHEET 18

MATCH TO SHEET 16

PLAN VIEW N



3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 064 22

CONSTRUCTION NOTES

- ① STRAND & LASH FIBER CABLES ALONG EXISTING UTILITY POLES.
- ⑤ PROPOSED (1) 2"Ø RISER ON EXISTING UTILITY POLE. PULL FIBER CABLES THROUGH.

POLE NUMBER	TP38840
NEUTRAL	29'3"
SECONDARY	28'5"
SECONDARY DRIP	27'4"
CLICK	22'2"
COMCAST	20'7"
TEL	18'3"
WAVE ATTACH	21'2"

PWR MR - LOWER CLICK TO 23'11"
MR - MOVE COMCAST TO 21'11"

POLE NUMBER	TP28279
SECONDARY	29'7"
NEUTRAL	28'11"
SL	27'9"
SECONDARY DRIP	27'3"
SL DRIP	26'0"
CLICK	25'3"
COMCAST	23'11"
WAVE ATTACH	22'11"

PWR MR - LOWER CLICK TO 22'8"
MR - MOVE COMCAST TO 20'8"

POLE NUMBER	TP33975
SECONDARY RACK	27'5"
SECONDARY PWR RISER	26'7"
SL	26'6"
SECONDARY DRIP	26'2"
SL DRIP	26'1"
CLICK	23'9"
COMCAST	22'8"
WAVE ATTACH	21'8"

MR - MOVE CLICK TO 24'10"

POLE NUMBER	TP23677
SECONDARY	31'5"
NEUTRAL	30'9"
SECONDARY PWR RISER	28'8"
SECONDARY DRIP	28'5"
CLICK	23'10"
COMCAST	22'9"
WAVE ATTACH	23'10"

PWR MR - RAISE SECONDARY DRIP TO 24'8"
PWR MR - LOWER CLICK TO 20'11"
MR - MOVE COMCAST TO 19'11"

POLE NUMBER	TP30330
NEUTRAL	25'3"
SECONDARY	24'8"
SECONDARY DRIP	24'1"
CLICK	21'10"
COMCAST	20'11"
TEL	19'2"
WAVE ATTACH	20'5"

POLE NUMBER	TP28311
NEUTRAL	31'9"
CLICK	23'3"
COMCAST	21'3"
TEL	20'2"
WAVE ATTACH	22'3"

POLE NUMBER	TP28275
NEUTRAL	28'1"
CLICK	24'9"
COMCAST	24'3"
WAVE ATTACH	24'3"

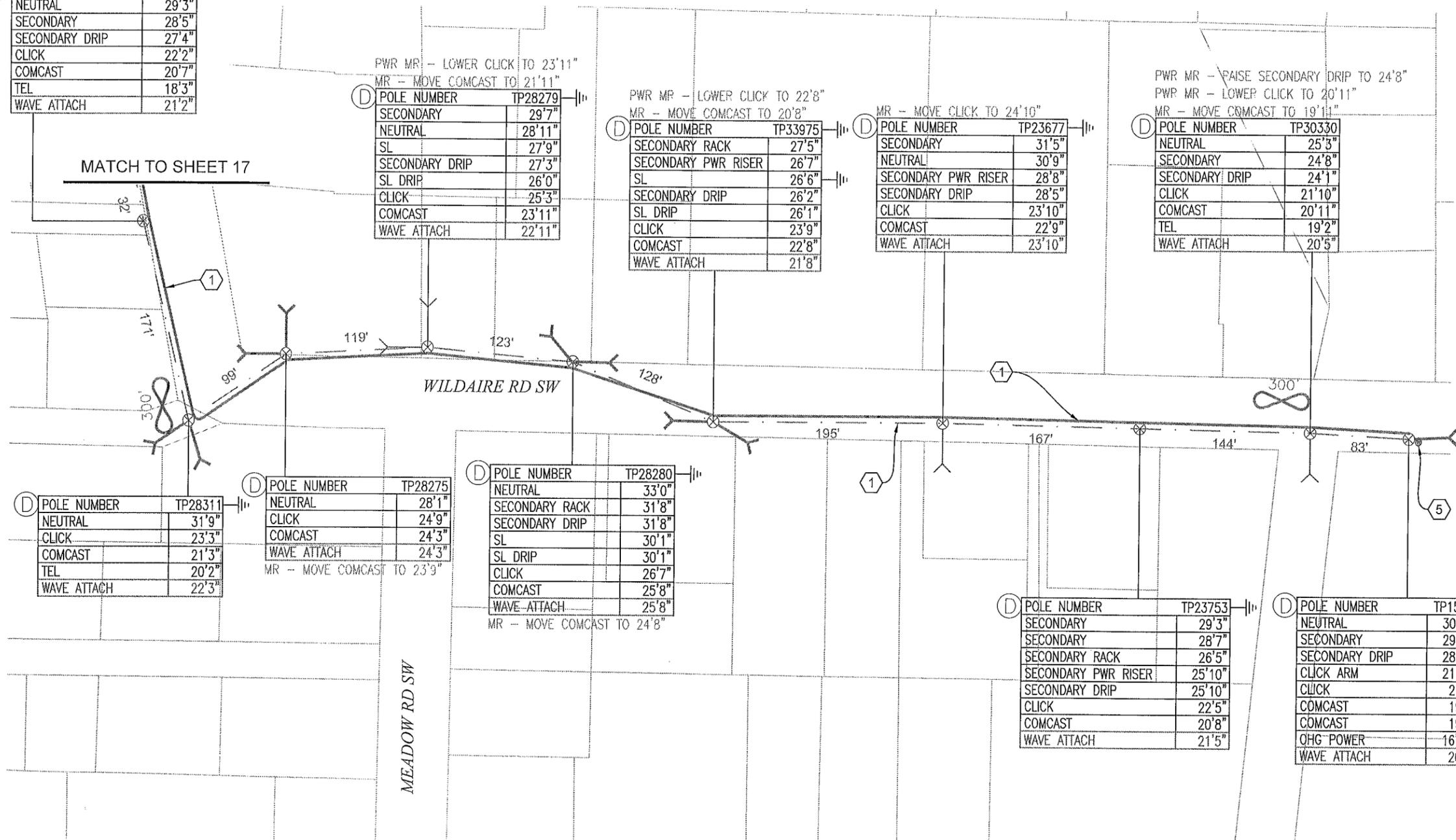
MR - MOVE COMCAST TO 23'9"

POLE NUMBER	TP28280
NEUTRAL	33'0"
SECONDARY RACK	31'8"
SECONDARY DRIP	31'8"
SL	30'1"
SL DRIP	30'1"
CLICK	26'7"
COMCAST	25'8"
WAVE ATTACH	25'8"

MR - MOVE COMCAST TO 24'8"

POLE NUMBER	TP23753
SECONDARY	29'3"
SECONDARY RACK	28'7"
SECONDARY PWR RISER	25'10"
SECONDARY DRIP	25'10"
CLICK	22'5"
COMCAST	20'8"
WAVE ATTACH	21'5"

POLE NUMBER	TP15631
NEUTRAL	30'10"
SECONDARY	29'10"
SECONDARY DRIP	28'10"
CLICK ARM	21'10"
CLICK	21'3"
COMCAST	19'9"
COMCAST	19'5"
OHG POWER	16'10"
WAVE ATTACH	20'3"



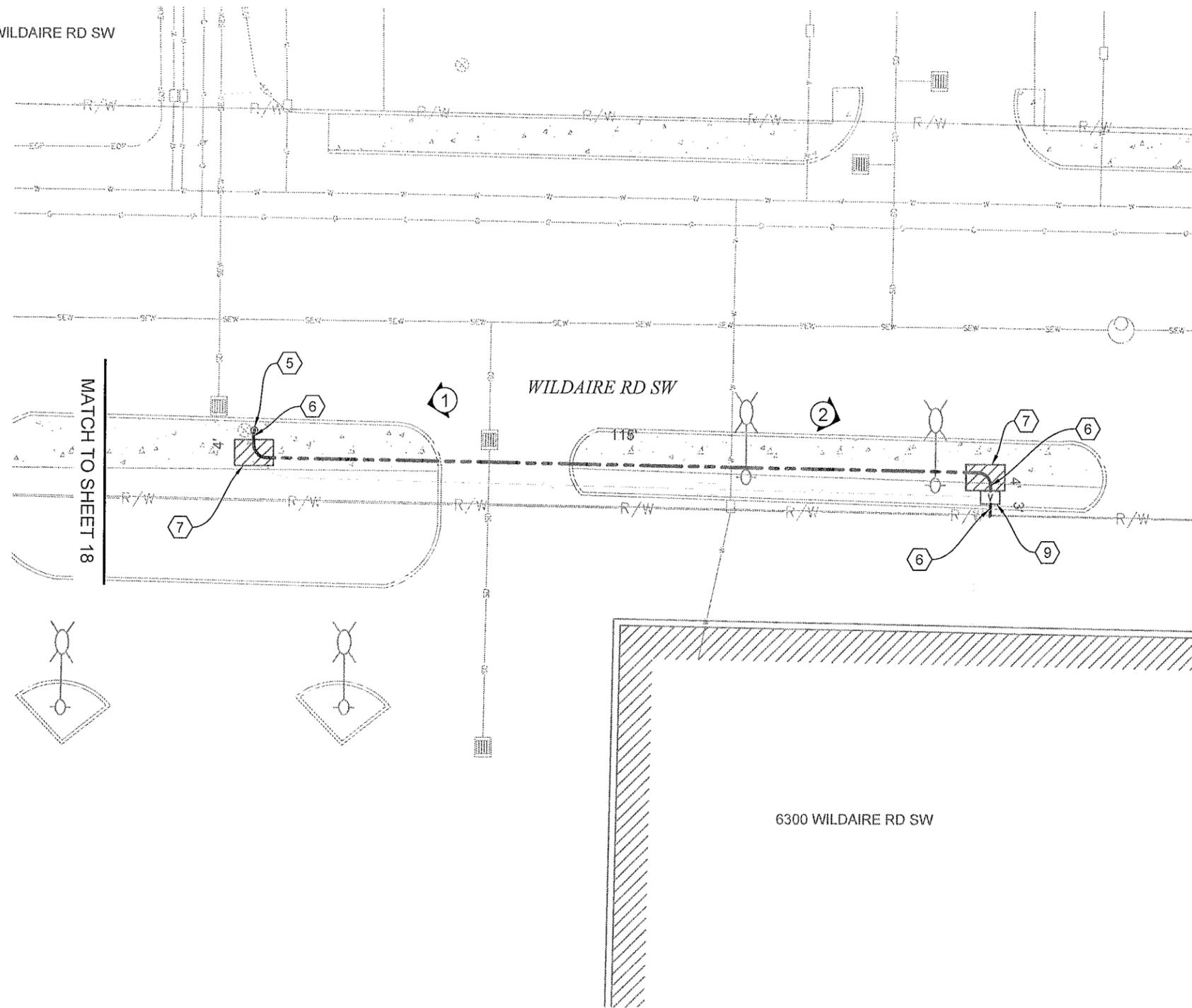
PLAN VIEW O

3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT

Wave **MGC**
TECHNICAL CONSULTING INC.

WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET: 065 22

6315 WILDAIRE RD SW



CONSTRUCTION NOTES

- 5 PROPOSED (1) 2"Ø RISER ON EXISTING UTILITY POLE. PULL FIBER CABLES THROUGH.
- 6 PROPOSED TRENCH (1) 2"Ø CONDUIT. PULL FIBER CABLES THROUGH. REPAIR SURFACE PER LOCAL JURISDICTION STANDARDS.
- 7 PROPOSED 4'X6' BORE PIT. REPAIR SURFACE PER LOCAL JURISDICTION STANDARDS.
- 8 PROPOSED BORE (1) 2"Ø CONDUIT. CONTRACTOR TO POT HOLE UTILITIES PRIOR TO BORE. PULL FIBER CABLES THROUGH.
- 9 PROPOSED (1) 24"X36"X24" VAULT. CORE VAULT AND PLACE CONDUIT FROM TRENCH. COIL FIBER CABLES THROUGH.

MATCH TO SHEET 18

WILDAIRE RD SW

6300 WILDAIRE RD SW

SYMBOL CORRESPONDS TO PHOTO LOCATIONS AND ORIENTATION. SEE SHEET 22 FOR SITE PHOTOGRAPHS.

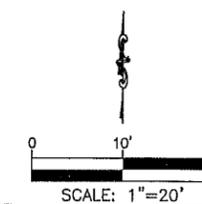
3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET 066 22

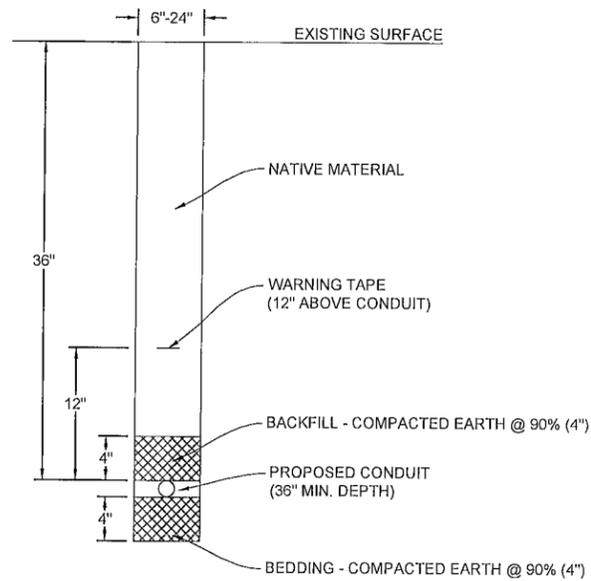


Know what's below.
Call before you dig.



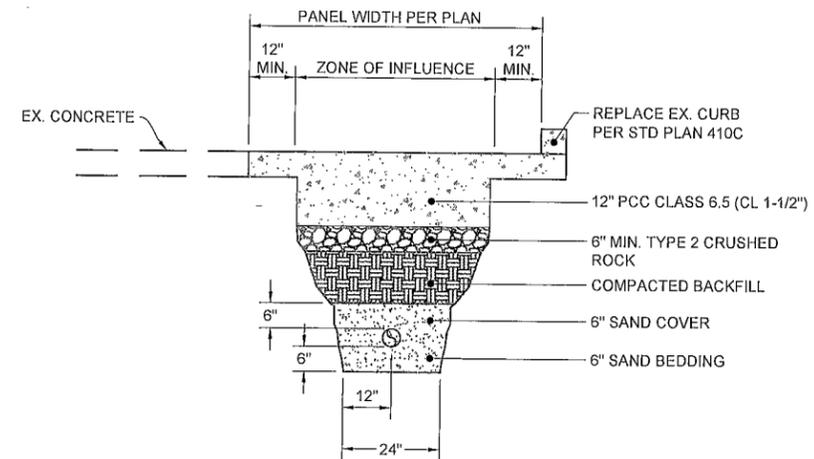
PLAN VIEW P

UNDERGROUND TYPICALS



TYPICAL TRENCH IN SOIL DETAIL

SCALE: NTS



TRENCH DETAIL A - TYPICAL CONCRETE TRENCH DETAIL

SCALE: NTS

FCA243624T-00006
FIBERGLASS / POLYMER CONCRETE ASSEMBLY

24" X 36" X 24"
(For actual dimensions see drawing)

Fiberglass / Polymer Concrete Assembly, Tapered Sides, No Floor, WUC 3.6, ANSISCTE 77-T16/20K, 3/8" Hex Bolts, Standard Manhole (Specify at time of order) Installed

LOAD RATINGS
Incidental Traffic - Parking Lot, Sidewalk
Conforms to:
• WUC 3.6
• ASTM C 857
• ANSISCTE 77

FEATURES:
• UL classified polymer concrete
• Drop-in manhole
• Shipped assembled
• SKH resistant cover
• Stainless steel bolts
• Cast-in floating nut box
• Integral drain holes

Additional product information continued on the reverse

newbasis
Composites for Infrastructure
2020 Kinross Avenue
Fremont, California 94507
851.787.0500
851.787.0532 (fax)
info@newbasis.com
newbasis.com

Inside Dimensions		
Length	Width	Depth
33 3/8"	21 1/2"	21"
44 1/4"	32 5/8"	

Rev: A.1

NB2436 VAULT DETAIL

SCALE: NTS



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3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
PROJECT NUMBER: 7484
LOCATION: 6300 WILDAIRE RD SW
LAKEWOOD WA, 98499
DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
WILDAIRE RD SW - LAKEWOOD.dwg
CONFIDENTIAL/PROPRIETARY SHEET: 068 22

SITE PHOTOGRAPHS

PHOTO #1: FACING WEST ON THE SOUTH SIDE OF WILDAIRE RD SW

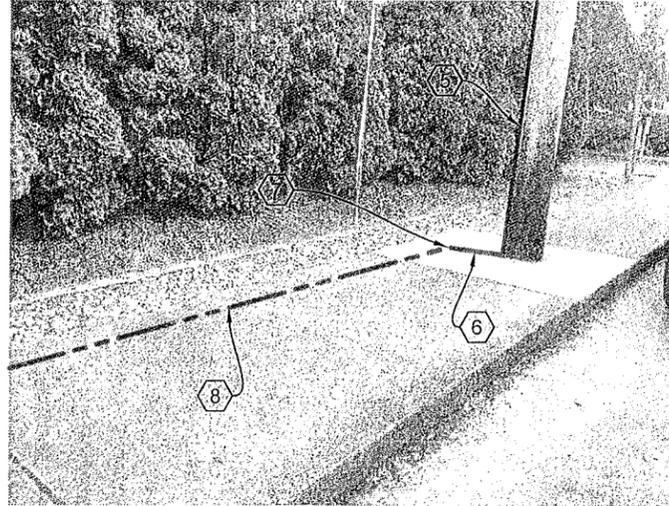


PHOTO #2: FACING EAST ON THE SOUTH SIDE OF WILDAIRE RD SW



CONSTRUCTION NOTES

- 5 PROPOSED (1) 2"Ø RISER ON EXISTING UTILITY POLE. PULL FIBER CABLES THROUGH.
- 6 PROPOSED TRENCH (1) 2"Ø CONDUIT. PULL FIBER CABLES THROUGH. REPAIR SURFACE PER LOCAL JURISDICTION STANDARDS.
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- 9 PROPOSED (1) 24"X36"X24" VAULT. CORE VAULT AND PLACE CONDUIT FROM TRENCH. COIL FIBER CABLES THROUGH.



Know what's below.
Call before you dig.

3				AS-BUILT
2				REVISION # 1
1	02/22/16	BB	NC	ORIGINAL
NO.	DATE	ENGINEER	DRAFTER	COMMENT



WAVE ENGINEER: CHRISTOPHER MANTLE
 ENGINEERING FIRM: MGC TECHNICAL CONSULTING INC.
 PROJECT NUMBER: 7484
 LOCATION: 6300 WILDAIRE RD SW
 LAKEWOOD WA, 98499
 DRAWING NAME: 7484-PLANS-PCLS LAKEWOOD LIBRARY - 6300
 WILDAIRE RD SW - LAKEWOOD.dwg
 CONFIDENTIAL/PROPRIETARY SHEET: 069 22

ORDINANCE NO. 643

AN ORDINANCE OF THE CITY OF LAKEWOOD GRANTING ASTOUND BROADBAND, LLC DBA WAVE A NON-EXCLUSIVE FRANCHISE FOR THE TRANSMISSION OF TELECOMMUNICATIONS IN, THROUGH, OVER AND UNDER THE RIGHTS-OF-WAY OF THE CITY OF LAKEWOOD.

WHEREAS, Astound Broadband, LLC, a Washington limited liability company DBA “Wave” (“Grantee”) has requested that the City grant it the right to install, operate and maintain a fiber optic-based telecommunications system within the public Rights-of-Way of the City; and

WHEREAS, the City Council has the authority under state law to grant franchises for the use of its Rights-of-Way; and

WHEREAS, the City Council finds it desirable for the welfare of the City and its residents that such a non-exclusive franchise be granted to Grantee; and

WHEREAS, the City is willing to grant the rights requested by Grantee subject to certain terms and conditions;

NOW, THEREFORE, The City Council of the City of Lakewood do ordain as follows:

Section 1. Definitions. Where used in this Ordinance and the franchise granted hereby (the "Franchise") these terms have the following meanings:

A. “Affiliate” means an entity which owns or controls, is owned or controlled by, or is under common ownership with Grantee.

B. "City” means the City of Lakewood, a municipal corporation of the State of Washington.

C. “Emergency Situation” means an emergency involving likely loss of life or substantial property damage as determined by City in good faith.

C. “Facilities” means Grantee’s fiber optic cable system constructed and operated within the City’s Rights-of-Way, and shall include all cables, wires, conduits, ducts, pedestals and any associated converter, equipment or other facilities within the City’s Rights-of-Way, designed and constructed for the purpose of providing Telecommunications Service and other lawful services not prohibited by this Ordinance.

D. “Franchise” shall mean the initial authorization or renewal thereof, granted by the City, through this Ordinance, or a subsequently adopted Ordinance, which authorizes construction and operation of the Grantee’s Facilities for the purpose of offering Telecommunications Service and other lawful services not prohibited by this Ordinance.

E. “Franchise Area” means the present municipal boundaries of the City, and shall include any additions thereto by annexation or other legal means.

F. “Person” means an individual, partnership, association, joint stock company, trust, corporation, limited liability company or governmental entity.

G. “Rights-of-Way” means the surface and the space above and below streets, roadways, highways, avenues, courts, lanes, alleys, sidewalks, rights-of-way and similar public areas within the City.

H. “Telecommunications Service” means any telecommunications service, telecommunications capacity, or dark fiber, provided by the Grantee using its Facilities, either directly or as a carrier for its Affiliates, or any other Person engaged in Telecommunications Services, including, but not limited to, the transmission of voice, data or other electronic information, facsimile reproduction, burglar alarm monitoring, meter reading and home shopping, or other subsequently developed technology that carries a signal over fiber optic cable. Telecommunications Service shall also include non-switched, dedicated and private line, high capacity fiber optic transmission services to firms, businesses or institutions within the City and other lawful services not prohibited by this Ordinance. However, Telecommunications Service shall not include the provision of “cable services”, as defined by 47 U.S.C. §522, as amended, for which a separate franchise would be required.

Section 2. Franchise Area and Authority Granted.

A. Facilities within Franchise Area. The City does hereby grant to Grantee the right, privilege, authority and franchise to construct, support, attach, connect and stretch Facilities between, maintain, repair, replace, enlarge, operate and use Facilities in, upon, over, under, along and across rights-of-way in the Franchise Area for purposes of telecommunications service as defined in RCW 82.04.065.

B. Permission Required to Enter Onto Other City Property. Nothing contained in this Ordinance is to be construed as granting permission to Grantee to go upon any other public place other than Rights-of-Way within the Franchise Area in this Ordinance. Permission to go upon any other property owned or controlled by the City must be sought on a case-by-case basis from the City.

C. Compliance with WUTC Regulations. At all times during the term of the Franchise, Grantee shall fully comply with all applicable regulations of the Washington Utilities and Transportation Commission.

Section 3. Construction and Maintenance.

A. Grantee's Facilities shall be located, relocated and maintained within the Rights-of-Way in accordance with Lakewood Municipal Code (“LMC”) and so as not to unreasonably interfere with the free and safe passage of pedestrian and vehicular traffic and ingress or egress

to or from the abutting property and in accordance with the laws of the State of Washington. Whenever it is necessary for Grantee, in the exercise of its rights under the Franchise, to make any excavation in the Rights-of-Way, Grantee shall obtain prior approval from the City of Lakewood Public Works Department, pay the applicable permit fees, and obtain any necessary permits for the excavation work pursuant to LMC. Upon completion of such excavation, Grantee shall restore the surface of the Rights-of-Way to the specifications established within the Lakewood Municipal Code and City of Lakewood Engineering Standards Manual. If Grantee should fail to leave any portion of the excavation in a condition that meets the City's specifications per the LMC and City Engineering Standards, the City may, on five (5) days' notice to Grantee, which notice shall not be required in case of an Emergency Situation, cause all work necessary to restore the excavation to a safe condition. Grantee shall pay to the City the reasonable cost of such work.

B. Any surface or subsurface failure occurring during the term of this Agreement caused by any excavation by Grantee shall be repaired to the City's specifications, within thirty (30) days, or, upon five (5) days written notice to Grantee, the City may order all work necessary to restore the damaged area to a safe and acceptable condition and Grantee shall pay the reasonable cost of such work to the City.

C. In the event of an Emergency Situation, Grantee may commence such emergency and repair work as required under the circumstances, provided that Grantee shall notify the City Public Works Director in writing as promptly as possible before such repair or emergency work commences, or as soon thereafter as possible, if advance notice is not reasonably possible. The City may act, at any time, without prior written notice in the case of an Emergency Situation, but shall notify Grantee in writing as promptly as possible under the circumstances.

D. Grantee agrees that if any of its actions under the Franchise materially impair or damage any City property, survey monument, or property owned by a third party, Grantee will restore, at its own cost and expense, the impaired or damaged property to the same condition as existed prior to such action. Such repair work shall be performed and completed to the reasonable satisfaction of the Public Works Director.

Section 4. Location and Relocation of Facilities.

A. Grantee shall place any new Facilities underground where existing telecommunications and cable facilities are located underground. Any new Facilities to be located above-ground shall be placed on existing utility poles. No new utility poles shall be installed in connection with placement of new above-ground Facilities.

B. Grantee recognizes the need for the City to maintain adequate width for installation and maintenance of sanitary sewer, water and storm drainage utilities owned by the City and other public utility providers. Thus, the City reserves the right to maintain clear zones within the public right-of-way for installation and maintenance of said utilities. The clear zones for each right-of-way segment shall be noted and conditioned with the issuance of each Right-of-Way permit. If adequate clear zones are unable to be achieved on a particular right-of-way, Grantee

shall locate in an alternate Right-of-Way, obtain easements from private property owners, or propose alternate construction methods which maintain and/or enhance the existing clear zones.

C. Except as otherwise required by law, Grantee agrees to relocate, remove or reroute its Facilities as ordered by the City, at no expense or liability to the City, except as may be required by RCW Chapter 35.99. Pursuant to the provisions of Section 5, Grantee agrees to protect and save harmless the City from any third party claims for service interruption or other losses in connection with any such change or relocation other than City's negligence or willful misconduct.

D. If the City determines that a project necessitates the relocation of the Grantee's existing Facilities, then:

1. Within a reasonable time, which shall be no less than ninety (90) days prior to the commencement of the project, the City shall provide the Grantee with written notice requiring relocation; provided that, in the event of an Emergency Situation beyond the control of the City and which will result in severe financial consequences to the City or its citizens or businesses, the City shall give the Grantee written notice as soon as practicable;

2. The City shall provide the Grantee with copies of information for such improvement project and a proposed location for the Grantee's Facilities so that Grantee may relocate its Facilities in other Rights-of-Way in order to accommodate the project; and

3. The Grantee shall complete relocation of its Facilities at no charge or expense to the City so as to accommodate the project at least ten (10) days prior to commencement of the project. In the event of an Emergency Situation as described in this Section, the Grantee shall relocate its Facilities within the reasonable time period specified by the City.

E. The Grantee may, after receipt of written notice requesting a relocation of its Facilities, submit to the City written alternatives to such relocation. The City shall evaluate such alternatives and advise the Grantee in writing if one or more of the alternatives are suitable to accommodate the work, which would otherwise necessitate relocation of the Facilities. If so requested by the City, the Grantee shall submit additional information to assist the City in making such evaluation. The City shall give each alternative proposed by the Grantee full and fair consideration, within a reasonable time, so as to allow for the relocation work to be performed in a timely manner. In the event the City ultimately determines that there is no other reasonable alternative, the Grantee shall relocate its Facilities as otherwise provided in this Section.

F. The provisions of this Section shall in no manner preclude or restrict the Grantee from making any arrangements it may deem appropriate when responding to a request for relocation of its Facilities by any Person or entity other than the City, where the Facilities to be constructed

by said Person or entity are not or will not become City-owned, operated or maintained Facilities; provided, that such arrangements shall not unduly delay a City construction project.

G. The Grantee shall indemnify, hold harmless and pay the costs of defending the City against any and all third party claims, suits, actions, damages, or liabilities for delays on City construction projects caused by or arising out of the failure of the Grantee to relocate its Facilities in a timely manner; provided, that the Grantee shall not be responsible for damages due to delays caused by the City or circumstances beyond the reasonable control of the Grantee.

H. In the event that the City orders the Grantee to relocate its Facilities for a project which is primarily for private benefit, the private party or parties causing the need for such project shall reimburse the Grantee for the cost of relocation in the same proportion as their contribution to the total cost of the project.

I. In the event of an unforeseen Emergency Situation that creates a threat to public safety, health or welfare, the City may require the Grantee to relocate its Facilities at its own expense, any other portion of this Section notwithstanding.

Section 5. Indemnification.

A. Grantee shall indemnify, defend and hold the City, its agents, officers, employees, volunteers and assigns harmless from and against any and all third party claims, demands, liability, loss, cost, damage or expense of any nature whatsoever, including all costs and reasonable attorney's fees, made against them on account of injury, sickness, death or damage to persons or property which is caused by or arises out of, in whole or in part, the willful, tortious or negligent acts, failures and/or omissions of Grantee or its agents, servants, employees, contractors, subcontractors or assigns in the construction, operation or maintenance of its Facilities or in exercising the rights granted Grantee in the Franchise; *provided, however*, such indemnification shall not extend to injury or damage caused by the negligence or willful misconduct of the City, its agents, officers, employees, volunteers or assigns.

B. In the event any such claim or demand be presented to or filed with the City, the City shall promptly notify Grantee thereof (and in any event prior to the date that Grantee's rights to defend such claim or demand would be prejudiced), and Grantee shall have the right, at its election and at its sole cost and expense, to settle and compromise such claim or demand, provided further, that in the event any suit or action be begun against the City based upon any such claim or demand, it shall likewise promptly notify Grantee thereof, and Grantee shall have the right, at its election and its sole cost and expense, to settle and compromise such suit or action, or defend the same at its sole cost and expense, by attorneys of its own election.

Section 6. Default.

A. If Grantee shall fail to comply with any of the provisions of the Franchise, unless otherwise provided in the Franchise, the City will serve upon Grantee a written order to comply within thirty (30) days from the date such order is received by Grantee. If Grantee is not in compliance with the Franchise after expiration of the thirty (30) day period, the City may act to

remedy the violation and may charge the reasonable costs and expenses of such action to Grantee. The City may act without the thirty (30) day notice in case of an Emergency Situation. If any failure to comply with the Franchise by Grantee cannot be corrected with due diligence within said thirty (30) day period, then the time within which Grantee may so comply shall be extended for such time as may be reasonably necessary and so long as Grantee works promptly and diligently to effect such compliance. During such a period, if Grantee is not in compliance with the Franchise, and is not proceeding with due diligence in accordance with this section to correct such failure to comply, then the City may in addition, by ordinance and following written notice to Grantee, declare an immediate forfeiture of the Franchise and all of Grantee's rights and obligations thereunder.

B. In addition to other remedies provided in this Franchise or otherwise available at law, if Grantee is not in compliance with requirements of the Franchise, and if a good faith dispute does not exist concerning such compliance, the City may place a moratorium on issuance of pending Grantee Right-of-Way use permits until compliance is achieved.

Section 7. Nonexclusive Franchise.

A. The Franchise granted by this Ordinance is not and shall not be deemed to be an exclusive franchise. The Franchise granted by this Ordinance shall not in any manner prohibit the City from granting other and further franchises over, upon, and along the Franchise Area. The Franchise granted by this Ordinance shall not prohibit or prevent the City from using the Franchise Area or affect the jurisdiction of the City over the same or any part thereof.

Section 8. Franchise Term.

A. Unless earlier terminated by Grantee upon notice to the City, the Franchise is and shall remain in full force and effect for a period of ten (10) years from and after the effective date of this Ordinance, provided that the term may be extended for an additional five (5) years upon the agreement of Grantee and the City; and provided further, however, Grantee shall have no rights under the Franchise nor shall Grantee be bound by the terms and conditions of the Franchise unless Grantee shall, within thirty (30) days after the effective date of this Ordinance, file with the City its written acceptance of the Franchise, in a form acceptable to the City Attorney.

B. If the City and Grantee fail to formally renew the Franchise prior to the expiration of its term or any extension thereof, the Franchise shall automatically continue in full force and effect until renewed or until either party gives written notice at least one hundred eighty (180) days in advance of intent not to renew the Franchise.

Section 9. Compliance with Codes and Regulations.

A. The rights, privileges and authority herein granted are subject to and governed by this Ordinance, the applicable laws of the State of Washington and the applicable laws of the United States, and all other applicable ordinances and codes of the City of Lakewood, as they now exist or may hereafter be amended, including but not limited to the provisions of Lakewood Municipal

Code. Nothing in this ordinance limits the City's lawful power to exercise its police power to protect the safety and welfare of the general public. Any location, relocation, erection or excavation by Grantee shall be performed by Grantee in accordance with applicable federal, state and city rules and regulations, including the City's Public Works Policies and Standard Plans, and any required permits, licenses or posted fees, and applicable safety standards then in effect.

B. In the event that any territory served by Grantee is annexed to the City after the effective date of the Franchise, such territory shall be governed by the terms and conditions contained herein upon the effective date of such annexation.

C. The City acknowledges that Washington law currently limits the tax the City may impose on Grantee's activities hereunder to 6% of revenue derived from the provision of network telephone service (i.e., "telephone business" as defined in RCW 82.16.010) and that the federal Internet Tax Freedom Act prohibits the imposition of a tax or other fee on revenue derived by Grantee from Grantee's provision of Internet access services. Grantee agrees that if federal or Washington law is changed, Grantee, following not less than ninety (90) days written notice from the City, will negotiate in good faith with the City to amend the Franchise to expand the revenue base on which such tax is applied.

Section 10. Undergrounding.

A. New Facilities shall be installed underground pursuant to Section 4 of the Franchise. Grantee acknowledges the City's policy of undergrounding of Facilities within the Franchise Area. Grantee will cooperate with the City in the undergrounding of Grantee's existing Facilities within the Franchise Area. If during the term of the Franchise, the City shall direct Grantee to underground Facilities within any Franchise Area, such undergrounding shall be at no cost to the City except as may be provided in RCW Chapter 35.99. Grantee shall comply with all federal, state, and City regulations on undergrounding. If the City undertakes any street improvement which would otherwise require relocation of Grantee's above-ground Facilities, the City may, by written notice to Grantee, direct that Grantee convert any such Facilities to underground Facilities.

Section 11. Record of Installations and Service.

A. With respect to excavations by Grantee and the City within the Franchise Area, Grantee and the City shall each comply with its respective obligations pursuant to Chapter 19.122 RCW and any other applicable state or federal law.

B. Upon written request of the City, Grantee shall provide the City with the most recent update available of any plan of potential improvements to its Facilities within the Franchise Area; provided, however, any such plan so submitted shall be for informational purposes within the Franchise Area, nor shall such plan be construed as a proposal to undertake any specific improvements within the Franchise Area.

C. As-built drawings and maps of the precise location of any Facilities placed by Grantee in any Rights-of-Way shall be made available by Grantee to the City within ten (10) working

days of the City's written request. These plans and maps shall be provided at no cost to the City and shall include hard copies and/or digital copies in a format commonly used in the telecommunications industry.

Section 12. Shared Use of Excavations and Trenches.

A. If either the City or Grantee shall at any time after installation of the Facilities plan to make excavations in the area covered by the Franchise and as described in this Section, the party planning such excavation shall afford the other, upon receipt of written request to do so, an opportunity to share such an excavation, *provided that*: (1) such joint use shall not unreasonably delay the work of the party causing the excavation to be made or unreasonably increase its costs; (2) such joint use shall be arranged and accomplished on terms and conditions satisfactory to both parties. In addition, pursuant to RCW 35.99.070, the City may request that Grantee install additional conduit, ducts and related access structures for the City pursuant to contract, under which Grantee shall recover its incremental costs of providing such facilities to the City.

B. The City reserves the right to not allow open trenching for five years following a street overlay or improvement project. Grantee shall be given written notice at least ninety (90) days prior to the commencement of the project. Required trenching due to an emergency will not be subject to five (5) year street trenching moratoria.

C. The City reserves the right to require Grantee to joint trench with other franchisees if both entities are anticipating trenching within the same franchise area and provided that the terms of this Section are met.

Section 13. Insurance.

A. Grantee shall procure and maintain for the duration of the Franchise, insurance against claims for injuries to persons or damage to property which may arise from or in connection with the performance of work under the Franchise by Grantee, its agents, representatives or employees in the amounts and types set forth below.

1. Commercial General Liability insurance with limits no less than \$5,000,000 combined single limit for bodily injury (including death) and property damage, including premises operation, products and completed operations and explosion, collapse and underground coverage extensions;

2. Automobile liability for owned, non-owned and hired vehicles with a combined single limit of \$3,000,000 for each accident for bodily injury and property damage; and

3. Worker's compensation within statutory limits and employer's liability insurance with limits of not less than \$1,000,000 for each accident/disease/policy limit or as required by law.

B. Grantee's insurance coverage shall be primary insurance as respects the City. Any insurance, self-insurance or insurance pool coverage maintained by the City shall be in excess of Grantee's insurance and shall not contribute with it.

C. Grantee shall furnish the City with certificates of the foregoing insurance coverage or a copy of amendatory endorsements, including but not necessarily limited to the additional insured endorsement.

D. Grantee shall have the right to self-insure any or all of the above-required insurance. Any such self-insurance is subject to approval by the City.

E. Grantee's maintenance of insurance as required by the Franchise shall not be construed to limit the liability of Grantee to the coverage provided by such insurance, or otherwise limit City's recourse to any remedy to which the City is otherwise entitled at law or in equity.

Section 14. Bonding Requirement

A. Before undertaking any of the work authorized by this Franchise, the Grantee shall furnish an ongoing performance bond executed by the Grantee and a corporate surety authorized to do surety business in the State of Washington, in a sum to be set and approved by the Public Works Director as reasonably sufficient to ensure performance of Grantee's obligations under this Franchise. The bond shall be conditioned so that Grantee shall observe all the covenants, terms and conditions and faithfully perform all of the obligations of this Franchise, and to restore or replace any defective work or materials discovered in the restoration of the Franchise Area within a period of two (2) years from the final City inspection date of any such restoration. Grantee may meet the obligations of this section with one (1) or more bonds with an –A VII rating or better. In the event that a bond furnished pursuant to this section is canceled by the surety, after proper notice and pursuant to the terms of said bond, the Grantee shall, prior to the expiration of said bond, procure a replacement bond which complies with the terms of this section.

B. With respect to undertaking any of the work authorized by this Franchise, in the event the Grantee fails to perform its obligations under this Franchise and further fails to cure any deficiency within a reasonable period of time after receipt of written notice of such deficiency by the City, then the City may use any bond(s) furnished by the Grantee pursuant to Section 14(A) to cure such deficiency. Neither the amount of such bond(s) nor the City's use thereof shall limit the City's full recovery from the Grantee of costs incurred by the City to cure such deficiency.

C. In the event the City makes use of such bond(s) furnished by the Grantee pursuant to Section 14 (B) the City shall promptly provide written notice of same to the Grantee. Within thirty (30) days of receipt of such notice, the Grantee shall replenish or replace such bond(s) as provided in Section 14 (A).

D. The rights reserved to the City by this Section 14 are in addition to other rights of the City whether reserved by this Franchise or authorized by law, and no action, proceeding, or

exercise of right under this Section 14 shall constitute an election or waiver of any rights or other remedies the City may have.

Section 15. Street Vacation.

A. If any Public right-of-way of portion thereof used by Grantee is to be vacated during the term of this Franchise, unless as a condition of such vacation the Grantee is granted the right to continue its Facilities in the vacated Public right-of-way, Grantee shall, without delay or expense to the City, remove its Facilities from such Public right-of-way, and restore, repair or reconstruct the Public right-of-way where such removal has occurred, and place the Public right-of-way in such condition as may be required by the City.

Section 16. Assignment.

A. All of the provisions, conditions, and requirements herein contained shall be binding upon Grantee, and no right, privilege, license or authorization granted to Grantee hereunder may be assigned or otherwise transferred without the prior written authorization and approval of the City, which the City may not unreasonably withhold. Notwithstanding the foregoing, Grantee, without the consent of, but upon notice to the City, may assign this agreement in whole or in part to: (a) an Affiliate (as defined in this Ordinance); or (b) a lender for security purposes only.

B. Grantee may lease the Facilities or any portion thereof to another or provide capacity or bandwidth in its Facilities to another, *provided that*: Grantee at all times retains exclusive control over such Facilities and remains responsible for locating, servicing, repairing, relocating or removing its Facilities pursuant to the terms and conditions of the Franchise.

Section 17. Abandonment and Removal of Facilities.

A. Upon the expiration, termination, or revocation of the rights granted under the Franchise, the Grantee shall remove all of its Facilities from the Rights-of-Way of the City within ninety (90) days of receiving notice from the City's Public Works Director; *provided however*, that the City may permit the Grantee's improvements to be abandoned in place in such a manner as the City may prescribe. Upon permanent abandonment, and Grantee's agreement to transfer ownership of the Facilities to the City, the Grantee shall submit to the City a proposal and instruments for transferring ownership to the City. Any such Facilities which are not permitted to be abandoned in place which are not removed within ninety (90) days of receipt of said notice shall automatically become the property of the City; *provided however*, that nothing contained within this Section shall prevent the City from compelling the Grantee to remove any such Facilities through judicial action when the City has not permitted the Grantee to abandon said Facilities in place.

Section 18. Dangerous Conditions, Authority for City to Abate.

A. In the event that Grantee's Facilities or operations cause or contribute to a condition that appears to endanger any person or substantially impair the lateral support of the Franchise Area or public or private property adjacent thereto, the Public Works Director may direct

Grantee, at no charge or expense to the City, to promptly take such action as may be reasonably necessary to resolve such condition to eliminate such endangerment. Such directive may include compliance within a prescribed period of time.

B. In the event Grantee fails to promptly take action as directed by the City pursuant to Section 18 (A), or fails to fully comply with such direction, or if emergency conditions exist which require immediate action to prevent imminent injury or damage to persons or property, the City may take action as it reasonably believes is necessary with respect to Grantee's Facilities or operations to protect persons or property and in such event Grantee shall be responsible to reimburse the City for its costs incurred in so doing.

Section 19. Miscellaneous.

A. If any term, provision, condition or portion of this Ordinance shall be held to be invalid, such invalidity shall not affect the validity of the remaining portions of this Ordinance which shall continue in full force and effect. The headings of sections and paragraphs of this Ordinance are for convenience of reference only and are not intended to restrict, affect, or be of any weight in the interpretation or construction of the provisions of such sections or paragraphs.

B. Grantee shall pay for the City's reasonable administrative costs in drafting and processing this Ordinance and all work related thereto, which payment shall not exceed \$2,000. Grantee shall further be subject to all published permit fees associated with activities and the provisions of any such permit, approval, license, agreement or other document, the provisions of the Franchise shall control.

C. Failure of the City to declare any breach or default under this Franchise or any delay in taking action shall not waive such breach or default, but the City shall have the right to declare any such breach or default at any time. Failure of the City to declare one breach or default does not act as a waiver of the City's right to declare another breach or default.

D. Notwithstanding anything to the contrary herein, any determination by the City with respect to matters contained in this Ordinance and matters related to the Franchise shall be made in accordance with applicable federal law, including without limitation any applicable rules and regulations promulgated by the Federal Communications Commission, applicable state law and in a reasonable and non-discriminatory manner.

E. Grantee will provide contact information to the City for purposes of including the Grantee in any coordination with local utility providers.

Section 20. Notice.

A. Any notice or information required or permitted to be given to the parties under this Franchise may be sent to the following addresses unless otherwise specified:

City of Lakewood: Public Works Director City of Lakewood 6000 Main Street SW Lakewood, WA 98499-5027	Grantee: Astound Broadband, LLC 401 Kirkland Parkplace, Suite 500 Kirkland, WA 98033 Attn: Steve Weed, CEO and Byron Springer, EVP
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Notice shall be deemed given upon receipt in the case of personal delivery, three (3) days after deposit in the United States Mail in the case of regular mail, or the next day in the case of overnight delivery.

Section 21. Alteration of Franchise.

A. The City and Grantee hereby reserve the right to alter, amend or modify the terms and conditions of this Franchise in accordance with the provisions of this Section.

B. At any time during the term of this Franchise, the City or Grantee may request, by written notice, that the other promptly participate in negotiations to alter, amend or modify the terms and conditions of this Franchise.

C. Within a reasonable time after receipt of the notice, the City and Grantee shall, at a mutually agreed-upon time and place, commence negotiations to alter, amend or modify the terms and conditions of this Franchise. The City and Grantee shall conduct such negotiations in good faith and with due regard to all pertinent facts and circumstances; provided, however, that neither the City nor Grantee shall be obligated to agree to any proposed alteration, amendment or modification. Further, no rights or privileges granted by this Franchise shall be prejudiced, impaired or otherwise affected by the failure of the City or Grantee to agree to any proposed alteration, amendment or modification.

D. Neither the City nor Grantee shall be obligated to continue negotiations after the expiration of ninety (90) days from the date they commence such negotiations; provided, however, the City and Grantee may agree to continue such negotiations for an additional period of time.

E. Any alteration, amendment or modification to which the City and Grantee agree shall be submitted to the legislative authority of the City as a proposed ordinance. The ordinance so proposed shall expressly provide that, unless Grantee properly files a written notice of acceptance within sixty (60) days of its effective date, the ordinance shall not be effective and this Franchise shall not be altered, amended or modified. To the extent permitted by law, the party proposing the alteration, amendment or modification shall bear all actual administrative costs directly related to approval thereof.

F. It is the understanding of the parties that this franchise is limited to facilities necessary to provide Telecommunications Service. The parties to this agreement acknowledge that if the Grantee endeavors to provide services or utilities beyond the scope of this agreement, such

additional services or utilities may be added to this franchise only by written addendum. Additional services or utilities may be subject to franchise fees, and state or local taxes as allowed by law.

Section 22. Effective date. This Ordinance, being in compliance with RCW 35A.47.040, shall be in force and effect thirty (30) days from and after its passage by the Lakewood City Council and publication in the summary form attached to the original of this ordinance and by this reference approved by the City Council.

Passed by majority vote of the Lakewood City Council in open meeting this 20th _____ day of June, 2016.

Signed in authentication thereof this _____ day of _____, 2016.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Alice M. Bush, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

City of Lakewood
Lakewood City Hall
6000 Main Street SW
Lakewood, WA 98499
(253) 589-2489

(Legal Notice)

June 21, 2016

**NOTICE OF ORDINANCE PASSED
BY LAKEWOOD CITY COUNCIL**

The following is a summary of an Ordinance passed by the City of Lakewood City Council on the 20th day of June 2016.

ORDINANCE NO. 643

AN ORDINANCE OF THE CITY OF LAKEWOOD GRANTING
ASTOUND BROADBAND, LLC DBA WAVE A NON-
EXCLUSIVE FRANCHISE FOR THE TRANSMISSION OF
TELECOMMUNICATIONS IN, THROUGH, OVER AND
UNDER THE RIGHTS-OF-WAY OF THE CITY OF
LAKEWOOD.

This Ordinance, being in compliance with RCW 35A.47.040, shall be in force and effect thirty (30) days from and after its passage by the Lakewood City Council and publication in the summary form attached to the original of this ordinance and by this reference approved by the City Council.

The full text of the Ordinance is available at the City Clerk's Office, Lakewood City Hall, 6000 Main Street SW, Lakewood, Washington 98499, (253) 589-2489. A copy will be mailed out upon request.

Alice M. Bush, MMC, City Clerk

Published in the Tacoma News Tribune: _____.

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: June 20, 2016	TITLE: A Resolution adopting the City of Lakewood Six Year Comprehensive Transportation Improvement Program 2017-2022	TYPE OF ACTION: — ORDINANCE <input checked="" type="checkbox"/> RESOLUTION NO. 2016-12 — MOTION — OTHER
REVIEW: May 9, 2016 Council presentation; and June 6, 2016 Public Hearing	ATTACHMENTS: Resolution 2016-12 Comprehensive Transportation Improvement Program 2017-2022	

SUBMITTED BY: Don E. Wickstrom, P.E., Public Works Director / City Engineer

RECOMMENDATION: It is recommended that the City Council adopt the City of Lakewood Six Year Comprehensive Transportation Improvement Program (2017-2022).

DISCUSSION: Chapter 35.77.010 RCW requires that the City annually update its Six Year Comprehensive Transportation Improvement Program (6-Year TIP) and file a copy with the Secretary of the Washington State Department of Transportation within 30 days of adoption. **(Continued on Page 2)**

ALTERNATIVE(S): Because the adoption of a Six Year Comprehensive Transportation Program is required by State statute, there is no practical alternative to its adoption if the City wishes to continue leveraging local dollars with State and Federal funds. Council may however modify or prioritize projects within the Program as deemed appropriate.

FISCAL IMPACT: The Program at over \$138 million is not fiscally balanced meaning the expenditures exceed the current anticipated revenues. Of the \$138 million in capital improvements, approximately \$34 million would need to come from City funds with the balance (\$104 million) from grants and other sources. The TIP is not required to be fiscally balanced and the real decision on implementing any project comes with the adoption of the City’s budget. Adoption of the Program makes projects eligible to receive State or Federal funds. In addition, projects that are identified in the 6-Year TIP can be included in any future Transportation Benefit District (TBD) funding proposals.

Prepared by	 _____ City Manager Review
Department Director	

AGENDA BILL

PAGE 2

DISCUSSION: (Continued from Page 1)

The primary objective of the Program is to produce a comprehensive program for the orderly development and preservation of the City's street system. Only those projects identified in the adopted Program are eligible for state or federal grant funding.

Adoption of the Program does not irreversibly commit the City of Lakewood to construct identified projects. Projects in the early years of the Program have, however, a higher probability that they will be constructed as scheduled, at least those with significant grant funding therein versus projects in the later years, which are subjected to more flexibility and may be accelerated, delayed, or canceled as funding and conditions change. The usual reasons for canceling a project are that it is either environmentally unacceptable or contrary to the best interests of the community as a whole or its funding just didn't materialize. The Program may also be revised by a majority of the City Council at any time, but only after a public hearing.

The 2017-2022 Program was reviewed by the Planning Commission. The Program was presented to City Council at their May 6, 2016 council meeting and a public hearing was conducted on June 6, 2016. Prior to said public hearing it was sent to the Chamber of Commerce, neighborhood associations, rotary clubs, utility providers, Pierce Transit, Clover Park School District, Clover Park Technical College, Pierce College, and other recognized groups and organizations in Lakewood to enlist any comments they may have on it. Staff also placed the program on the City's web site.

During the Public Hearing one comment was received:

Michael Brandstetter, Lakewood resident, spoke about a citizen's comment that was provided before the Council tonight about the need for a crosswalk at Lakewood Drive between 100th Street and Steilacoom Boulevard and supported it being in the Six Year Transportation Improvement Program.

Staff Response: The citizen referenced by Mr. Brandstetter, who commented during the general comment period of the Council meeting, had contact the City of Lakewood's Public Works department on May 25, 2016 and requested a signalized crosswalk on Lakewood Dr. SW at Redwood Dr. SW where the new Four Heroes elementary was recently constructed. City staff formally responded to this request on June 3, 2016.

This location at Redwood Dr. SW has been analyzed for a signalized intersection and was analyzed during the permitting process for the Four Heroes School. There are many reasons for not adding a signalized crossing; first Lakewood drive is a principal arterial, adding a signal 370 feet south of the intersection of Steilacoom Blvd. SW would reduce the level of service for the roadway. Second Four Heroes Elementary does not service students to the west of Lakewood Dr. SW, as the area is serviced by the Park Lodge elementary school. As such it is Public Works recommendation not to incorporate the requested signal project into the 6-Year TIP.

RESOLUTION NO. 2016-12

A RESOLUTION of the City Council of the City of Lakewood, Washington, adopting a Six-Year Comprehensive Transportation Improvement Program for 2017 through 2022.

WHEREAS, pursuant to Section 35.77.010 of the Revised Code of Washington, cities are required to adopt and annually update a six-year Transportation Improvement Program as part of a coordinated transportation program; and,

WHEREAS, a public hearing was held before the Lakewood City Council at a meeting on June 6, 2016, to hear and receive public comment on the proposed, updated six-year program; and,

WHEREAS, the 2017-2022 Program was developed under the guidance of the Planning Commission and was sent to the Chamber of Commerce, neighborhood associations, Rotary clubs, utility providers, Pierce Transit, Clover Park School District, Clover Park Technical College, Pierce College, and other recognized groups and organizations in Lakewood soliciting comments prior to the public hearing; and,

WHEREAS, after considering all of the information presented the City Council finds that prioritized road and street capital improvement projects are essential to proper planning and the improvement of transportation within the City, and that planning as provided under the RCW is necessary to obtain available state and federal funding.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD,
WASHINGTON HEREBY RESOLVES, as Follows:

Section 1. That the Six-Year Comprehensive Transportation Improvement Program for 2017 through 2022, a copy of which is attached hereto and incorporated by reference, is adopted, and that the City Council authorizes the City Manager or designee to use the same in applications for grant funding for transportation related projects, and further authorizes the City Manager or designee to apply for such grants based thereon.

Section 2. That this Resolution shall be in full force and effect upon passage and signatures herein.

PASSED by the City Council this 20th day of June, 2016.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Alice M. Bush, MMC, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney



CITY OF LAKEWOOD
SIX-YEAR COMPREHENSIVE
TRANSPORTATION IMPROVEMENT PROGRAM
2017-2022

PREFACE

Chapters 35.77.010 of the Revised Code of Washington (RCW) provide that each city shall annually update its Six-Year Comprehensive Transportation Program (Program) and file a copy of the adopted Program with the Secretary of the Washington State Department of Transportation (WSDOT) by July 1 of each year. The Program is necessary to allow cities and counties to obtain State and Federal funding. For a project to obtain funding from the State, it must appear in the agency's current Program. Because the state also disperses federal highway funds, this requirement applies to federally funded projects as well.

RCW 35.77.010 also requires each city to specifically set forth those projects and programs of regional significance for inclusion in the transportation improvement program for that region.

The Program is based upon anticipated revenues versus desirable projects. There are always more projects than available revenues. Therefore, a primary objective of the Program is to integrate the two to produce a comprehensive, realistic program for the orderly development and preservation of our street system.

Several important points must be considered during the review of the proposed Program. The early years of the Program are fairly definite; that is, it can be assumed that those projects will be constructed as scheduled. Projects in the later years are more flexible and may be accelerated, delayed or canceled as funding and conditions change.

It is also important to note that the adoption of the Program does not irreversibly commit the City of Lakewood to construct the projects. A project may be canceled at any time during the course of study or design. The usual reasons for canceling a project are that it is environmentally unacceptable or contrary to the best interests of the community as a whole. The Program may at any time be revised by a majority of the City Council, but only after a public hearing.

CONSISTENCY WITH LAND USE MANAGEMENT PLAN

The State's Growth Management Act (GMA) requires local governments to develop and adopt comprehensive plans covering land use, housing, capital facilities, utilities, and transportation. These comprehensive plans must balance the demands of growth with the provision of public facilities and services and, in particular, transportation facilities and services. The City of Lakewood was required to develop and adopt a comprehensive plan that is in conformance with the requirements of the GMA.

The City of Lakewood has, as part of its Comprehensive Plan, a Transportation Element with a Master Goal to "Ensure that the transportation and circulation system is safe, efficient and serves all segments of the population and reduces reliance on single-occupant vehicles and increase use of other modes of transportation."

Specific goals include the following.

1. To provide a safe, comfortable and reliable transportation system.
2. To reduce consumption of energy through an efficient and convenient transportation system.
3. To enhance options for future improvements to the transportation system by taking advantage of advances in technology and transportation research.
4. To keep travel times for people and goods as low as possible.
5. To emphasize the movement of people and goods, rather than vehicles, in order to obtain the most efficient use of transportation facilities.
6. To establish a minimum level of adequacy for transportation facilities through the use of consistent and uniform standards.
7. To protect the capital investment in the transportation system through adequate maintenance and preservation of facilities.

The projects in the Six-Year Comprehensive Transportation Program are intended to conform to the goals within the City's Comprehensive Plan.

GRANT APPLICATIONS AND LEVERAGING LOCAL DOLLARS

The need to leverage local dollars through grant applications is very important to the City, especially in light of the decrease in funding available for transportation related capital improvements. The intent of this Program is not only to list and program projects for funding, but to establish City Council approval to submit grant applications on those projects contained in the Program.

FUNDING SOURCES

A. Motor Vehicle Fuel Tax Funds

The Motor Vehicle Fuel Tax Funds have been programmed to provide matching funds for federal aid and urban arterial projects and for projects to be implemented with Motor Vehicle Fuel Tax Funds only.

By law, each city receives a proportionate share of the total state motor vehicle fuel tax. Money received is a monthly allocation based on population. The dollars shown in this year's Program reflect the revenues from this source expected to be received by the City of Lakewood. It is anticipated that revenue received from gas tax for the Streets Capital Projects Fund will be: \$335,000 (FY 2015).

B. Federal Aid Funding Programs

Each of the Federal aid programs listed below has specific requirements a project must meet to qualify for funding under the individual program. For a project to receive funding from any of these sources it must compete with other public agency projects.

On December 4, 2015, President Obama signed the Fixing America's Surface Transportation Act (FAST ACT). The Act authorizes \$305 billion over fiscal years 2016 through 2020 for the Department's highway, highway and motor vehicle safety, public transportation, motor carrier safety, hazardous materials safety, rail, and research, technology and statistics programs. The ACT essentially continues on with a number of specific funding programs that were funded under the previous Federal Transportation program (MAP 21). These include the following:

1. STP Surface Transportation Program: This is a regionally competitive program.
2. CMAQ Congestion Mitigation and Air Quality: This is a regionally competitive program intended for projects that significantly improve air quality.
3. HSIP Highway Safety Improvement Program: Statewide competition for federal funds targeted at safety improvements at high accident locations.
4. TAP Transportation Alternatives Program: This is a regionally competitive program and focuses on pedestrian and bicycle facilities (on and off road); safe-routes to schools, etc.; and other non-highway focused programs.

Much of the above said Federal grant funds are funneled thru the regional MPOs which for Lakewood that's Puget Sound Regional Council (PSRC). Presently PSRC has call out for projects where in \$206,000,000 in grant funding is presently (January 1st 2017) available throughout its 4 county region. Typically Lakewood projects are most competitive at County Wide level for which under this present call there is \$27,480,000.

C. Washington State Transportation Improvement Board (TIB)

The TIB has a number of statewide competitive programs which use criteria developed by the TIB for prioritization of projects. The tow TIB programs in which the City can compete are as follows:

1. UAP. Urban Arterial Program. This program is for arterial street construction with primary emphasis on safety and mobility.
2. SP Sidewalk Program. This program is for the improvement of pedestrian safety, and to address pedestrian system continuity and connectivity.

D. Community Development Block Grants (CDBG)

This is a program to provide physical improvements within low-income census tracts or to promote economic development within the City. Through the years 2016-2021 it is anticipated that a minimum of \$300,000 (on average) per year will be made available for pavement preservation, street lighting, and pedestrian improvements in eligible neighborhoods.

E. City Funding Sources

1. Real Estate Excise Tax (REET). This funding source comes from the two ¼% REET's charged by the City on the sale of real estate within the City limits. The City's REET is designated entirely for transportation related capital improvements. Revenue from REET has averaged around \$900,000 in the past few years. The REET is estimated to be \$900,000 annually.
2. General Fund Transfer In. This funding source comes from several different sources that make up the General Fund revenue including: property tax, sales tax, and utility tax and fees. The Street Capital Projects Fund is budgeted to receive approximately \$500,000 annually (on average) over the next 5 years in support of the pavement preservation program.
3. Transportation Benefit District (TBD). In 2014, the TBD Board implemented a \$20 per vehicle tab fee to provide funds toward a specific list of pavement preservation projects to be implemented between 2015 through 2020. The anticipated revenue is approximately \$680,000 per year.

F. Washington State Department of Transportation

1. Pedestrian and Bicycle Program: This is a statewide competitive program specifically oriented toward the elimination of hazards to the pedestrian and bicyclists. The recent call for projects has expanded the program's scope to emphasize

“complete streets” – accommodation of all roadway users from vehicles to bicyclists to pedestrians. The programs focus for “complete streets” is for “main street” urban arterials and corridors. Historically, the city has not received much funding from this program. However, given the change in the grant scope, there may be opportunities from this source in the future.

2. Safe Routes to Schools Program: This is a statewide competitive program specifically oriented toward pedestrian and bicycle safety near schools.
3. Surface Water Management Program:

The City’s Surface Water Management (SWM) Program pays for all drainage facilities constructed in conjunction with street improvements. The revenue from SWM is directly related to the amount of capital improvement projects constructed. SWM participation in roadway projects averages about \$300,000 annually.

PROJECT NUMBERING SYSTEM

Project numbers within most sections of the Program are discontinuous in order to maintain consistency in project numbering from year to year.

Completed projects are removed from subsequent years' programs, thereby eliminating some project numbers.

Projects carried forward from previous year(s) retain the same project numbers from the previous year(s).

BUDGET DOLLARS

Costs shown are planning level estimates and are reflected in each year as FY2016 dollars with no accounting for inflation.

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		NOTE: BOLD and ITALICIZED numbers denote grant is secured							TOTAL FUNDS
SECTION 1			2017	2018	2019	2020	2021	2022	2017-2022
NEW CONSTRUCTION									
ARTERIAL STREET PROJECTS									
1.2 Gravelly Lake Drive @ I-5 Right Turn Lane	Widen GLD from Nyanza to I-5 SB on-ramp to provide dedicated right-turn lane. Traffic signal upgrades; bridge widening; r/w acquisition.	City				50	350		400
Total Estimated Cost \$1,600		Grant				200	1,000		1,200
		Other							
		Total		0	0	0	250	1350	0
1.4 Union Avenue - W. Thorne Ln. to Spruce St.	Widen to add turn lane, shared bike/travel lane, sidewalks, street lighting. Intersection improvements. <i>Notes: Limits revised to reflect recent improvements at Berkeley/Union.</i>	City					125	250	375
Total Estimated Cost \$5,000		Grant					375	2,250	2,625
		Other					75	150	225
		Total		0	0	0	0	575	2,650
1.18 96th Street - 2-way left turn lane	Widen 96th St. from 500' east of So. Tac. Wy to I-5 underpass to provide 2-way left turn lane. Does not include sidewalks or HMA overlay.	City					100		100
Total Estimated Cost \$500		Grant							0
		Other					400		400
		Total		0	0	0	0	500	0
1.20 123rd ST SW - Realignment	Realign 123rd ST SW as it enters Bridgeport	City					300		300
Total Estimated Cost \$400		Grant							0
		Other					100		100
		Total		0	0	0	0	400	0
1.21 Murray Road and 150th Street Corridor Capacity	Provide capacity for Woodbrook Industrial development: widening of Murray Road and 150th; bike/pedestrian facilities; structural pavement section improvements <i>Notes: Assume multiple phases; multiple years</i>	City		100	100	100			300
		Grant							0
		Other		1,500	1,500	1,500			4,500
		Total		0	1,600	1,600	1,600	0	0
1.22 Gravelly to Thorne Connector	Two-way connector road between Tillicum and Gravelly Lake Drive. Signalization.	City	1	1	1	1	1	1	6
Total Estimated Cost \$25,000		Grant							0
		Other		1,000	12,000	12,000			25,000
		Total		1	1,001	12,001	12,001	1	1
1.23 Interstate 5 through Lakewood (WSDOT led project - coordination only)	Planning and design coordination only.	City	1	1	1	1	1	1	6
		Grant							0
		Other							0
		Total		1	1	1	1	1	1

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		NOTE: BOLD and ITALICIZED numbers denote grant is secured							TOTAL FUNDS
SECTION 2 ROADWAY IMPROVEMENTS			2017	2018	2019	2020	2021	2022	2017-2022
2.26 Safety Improvements in the Vicinity of Schools	May include sidewalks, crossing improvements, signage, etc. in vicinity of schools.	City	100	150	30	100	250		630
		Grant	250	1,100	120	250	1,100		2,820
		Other		150			150		300
		Total	350	1,400	150	350	1,500	0	3,750
2.29 Steilacoom Blvd. Custer to 88th Street Total Estimated Cost \$1,975	Curbs, gutters, sidewalks, street lighting, on both sides. Signal modifications. Signal replacement Custer/Ardmore. Overlay.	City	0						0
		Grant	1,400						1,400
		Other	250						250
		Total	1,650	0	0	0	0	0	1,650
2.41 Steilacoom Blvd - Bridgeport Way to Fairlawn Total Estimated Cost \$1,400 <i>Note: Preliminary design completed via previous TIB grant</i>	Curbs, gutters, sidewalks, on both sides. Overlay.	City			10	20	100		130
		Grant			20	150	1,000		1,170
		Other					100		100
		Total	0	0	30	170	1,200	0	1,400
2.50 Gravelly Lake Drive - 100th to Bridgeport Way <i>Note: grant for design, environ., & r/w FY2011-2014</i>	Curb, gutters, sidewalks, street lighting, drainage. Signal modifications. Signal replacement Mt. Tacoma.	City	36						36
		Grant	1358						1,358
		Other	250						250
		Total	1,644	0	0	0	0	0	1,644
2.54 Minor Pedestrian Safety Improvements	Non-hardscape improvements. Shoulder widening on high-volume roads where less than 2' walkway exists.	City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total	50	50	50	50	50	50	300
2.55 High Accident Location Safety Improvements <i>2016-2017 Funds reallocated to 2.81 Roadway Safety Improvements to 40th Ave. SW and 96th St. SW and 3.20 Military Rd. and 112th St. Safety Improvement.</i>	May include sight distance corrective measures, signal modifications, etc. at one of top 25 accident locations.	City	44	20	49	50	50	50	263
		Grant							0
		Other							0
		Total	44	20	49	50	50	50	263
2.60 South Tacoma Way - SR512 to 96th Street Total Estimated Cost \$3,460 <i>Note: Design starting FY2011</i>	Curb, gutter, sidewalks, street lighting, drainage, overlay.	City	10						10
		Grant	50						50
		Other	10						10
		Total	70	0	0	0	0	0	70

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		<i>NOTE: BOLD and ITALICIZED numbers denote grant is secured</i>							TOTAL FUNDS
SECTION 2 ROADWAY IMPROVEMENTS			2017	2018	2019	2020	2021	2022	2017-2022
2.61 ADA Standards - Sidewalk Upgrades	On-going program to gradually upgrade existing facilities to current ADA standards	City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total	50	50	50	50	50	50	300
2.65 Steilacoom Blvd - 87th to 83rd <i>Design through project 2.74</i>	Curb, gutter, sidewalks, street lighting, drainage, overlay.	City		80	200				280
		Grant		200	1,400				1,600
		Other			200				200
		Total	0	280	1,800	0	0	0	2,080
2.66 Steilacoom Blvd - 83rd to Weller Road <i>Design through project 2.74</i>	Curb, gutter, sidewalks, street lighting, drainage, overlay.	City			70	200			270
		Grant			180	2,000			2,180
		Other				200			200
		Total	0	0	250	2,400	0	0	2,650
2.67 Bridgeport Way - I-5 to JBLM Gate Total Estimated Cost \$3,650	Curb, gutters, sidewalks, street lighting, drainage, overlay.	City	5						5
		Grant	10						10
		Other	5						5
		Total	20	0	0	0	0	0	20
2.68 Hipkins Rd. 104th to Steilacoom Blvd. Total Estimated Cost \$3,050	Curb, gutters, sidewalks, street lighting, drainage, overlay.	City							0
		Grant							0
		Other				350	2,700		3,050
		Total	0	0	0	350	2,700	0	3,050
2.69A Gravelly Lake Drive - 59th to Steilacoom Sidewalks	Curb, gutter, sidewalks on both sides of road.	City	80						80
		Grant	320						320
		Other							0
		Total	400	0	0	0	0	0	400
<i>Note: Project 2.69 split into two phases. TIB grant received for 2.69A.</i>									
2.69B Gravelly Lake Drive - Bridgeport to Steilacoom Road Diet Amendment 1: Project 2.69 split into two phases.	Reduce 4 travel lanes to 3. Curb, gutters, sidewalks, bike lanes, street lighting, drainage, overlay.	City	50	200					200
		Grant	100	1,100					1,100
		Other							0
		Total	150	1,300	0	0	0	0	1,300

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PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		NOTE: BOLD and ITALICIZED numbers denote grant is secured							TOTAL FUNDS
SECTION 2 ROADWAY IMPROVEMENTS			2017	2018	2019	2020	2021	2022	2017-2022
2.70 Lakewood Station - Non-Motorized Access Improvements	Curb, gutters, sidewalks, and street lighting improvements per Lakewood NMTP and Sound Transit Access Improvement Study.	City				100	100	100	300
		Grant			100	400	400	400	1,300
		Other			100	500	500	500	1,600
		Total	0	0	200	1,000	1,000	1,000	3,200
2.71A Steilacoom Blvd - Weller Road to Phillips Road - Ph. 1 <i>Design and right-of-way through project 2.74</i>	Curb, gutter, sidewalks, bikeway/buffer, street lighting, drainage on north side.	City		100					100
		Grant		450					450
		Other							0
		Total	0	550	0	0	0	0	550
2.71B Steilacoom Blvd - Weller Road to Phillips Road - Ph. 2 <i>Design through project 2.74</i>	Curb, gutter, sidewalks, bikeway, street lighting, drainage, overlay.	City		100					100
		Grant		1350					1,350
		Other		100					100
		Total	0	1,550	0	0	0	0	1,550
2.72 100th Street & Lakewood Drive <i>Bridgeport Way to 400 feet north of 100th Street</i>	Curb, gutter, sidewalks, sharrows, replace 100th/Lakewood signal, street lighting, drainage, overlay.	City	130	200					330
		Grant	550	800					1,350
		Other							0
		Total	680	1,000	0	0	0	0	1,680
2.73 112th / 111th - Bridgeport to Kendrick	Curb, gutter, sidewalks, sharrows, street lighting, drainage, overlay.	City	20	5	110				135
		Grant	100	50	1,440				1,590
		Other	50	45	250				345
		Total	170	100	1,800	0	0	0	2,070
2.74 Steilacoom Blvd Corridor Design - Farwest to Phillips <i>Joint project with Town of Steilacoom - DESIGN ONLY</i>	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City	50	43	43	14			150
		Grant	216	150	150	35			551
		Other	25	20	20	6			71
		Total	291	213	213	55	0	0	772
2.75 South Tacoma Way - 88th to North City Limits	Curb, gutter, sidewalks, bike lanes, street lighting, signal at 84th, drainage, overlay.	City	50	50	300				400
		Grant	150	150	2,341				2,641
		Other			300				300
		Total	200	200	2,941	0	0	0	3,341

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									
<i>NOTE: BOLD and ITALICIZED numbers denote grant is secured</i>									
SECTION 2			2017	2018	2019	2020	2021	2022	TOTAL FUNDS
ROADWAY IMPROVEMENTS									2017-2022
2.76A Phillips Road - Steilacoom to Hudtloff Middle School <i>Notes: Project 2.76 split into two phases. Safe Routes to Schools Grant received for 2.76A.</i>	Curb, gutter, sidewalks, bike lanes, street lighting, flashing beacons, drainage, on east side of road.	City	170						170
		Grant	480						480
		Other							0
		Total	650	0	0	0	0	0	650
2.76B Phillips Road - Steilacoom to Onyx <i>Notes: Project 2.76 split into two phases. Safe Routes to Schools Grant received for 2.76A.</i>	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City							0
		Grant							0
		Other			250	1850			2,100
		Total	0	0	250	1,850	0	0	2,100
2.77 Washington Blvd - Edgewood Ave to Gravelly Lake Drive <i>Notes: moves up project schedule in anticipation of grant.</i>	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City							0
		Grant	5,000						5,000
		Other	700						700
		Total	5,700	0	0	0	0	0	5,700
2.78 Oakbrook Sidewalks & Street Lighting Onyx Dr W (97th to 87th); Onyx Dr E (Garnet to Phillips) (Total Cost \$3,400)	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City							0
		Grant							0
		Other			400	3000			3,400
		Total	0	0	400	3,000	0	0	3,400
2.79 Lake City Business District Sidewalks (American Lake Park to Veterans Dr / Alameda) (Total Cost \$2,100)	Curb, gutter, sidewalks, sharrows, street lighting, drainage, overlay.	City							0
		Grant							0
		Other		300	1,800				2,100
		Total	0	300	1,800	0	0	0	2,100
2.80 Interlaaken Drive SW / Mt. Tacoma Drive Non-Motorized Improvements - Short Lane to Whitman Avenue SW (Total Cost Mt. Tacoma Drive \$2,950) (Total Cost Interlaaken \$4,000) formerly project 5.7. Construction 2022+	Provide curb and gutter, sidewalk and a shared travel/bike lane on one side of Interlaaken / Mt. Tacoma Dr.	City							
		Grant							
		Other					750	700	
		Total	0	0	0	0	750	700	0
2.81 Roadway Safety Improvements at 40th Ave. SW and 96th St. SW	Curb, gutter, sidewalk, sharrows, guard rail, street lighting, pavement reconstruction	City	15	1					16
		Grant	140	653					793
		Other							0
		Total	155	654	0	0	0	0	809

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		NOTE: BOLD and ITALICIZED numbers denote grant is secured							TOTAL FUNDS
SECTION 2 ROADWAY IMPROVEMENTS			2017	2018	2019	2020	2021	2022	2017-2022
2.82 59th Ave SW Sidewalk - 100th to Bridgeport Wy SW	Sidewalk east side of roadway.	City		25					25
		Grant		100					100
		Other							0
		Total	0	125	0	0	0	0	125
2.83 Gravelly Lake Dr. - Pacific Hwy to Nyanza (south)	Curb, gutter, sidewalk, bike way, street lighting, pavement rehab	City			50	75	250		375
		Grant			100	175	800		1,075
		Other							0
		Total	0	0	150	250	1,050	0	1,450
2.84 Lakewood Drive - Steilacoom Blvd to 74th Street <i>*note: pavement rehab City match also listed in project 9.15</i>	Add turn lanes, curb, gutter, sidewalk, bike way, street lighting, pavement rehab	City	50	100	950				1,100
		Grant	200	300	3,180				3,680
		Other	50	50	500				600
		Total	300	450	4,630	0	0	0	5,380
2.85 John Dower Road - Steilacoom to Custer <i>Notes: Project added. Safe Routes to Schools Grant received.</i>	Curb, gutter, sidewalk, bike way, street lighting, flashing crossing beacons, drainage, pavement.	City	150						150
		Grant	500						500
		Other							0
		Total	650	0	0	0	0	0	650
2.86 Veterans Drive SW-Gravelly Lake Dr. to Alameda (Total)	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay	City						860	860
		Grant						3,440	3,440
		Other							0
		Total	0	0	0	0	0	4,300	4,300
2.87 Steilacoom Blvd. SW - Phillips Rd. SW to 88th St. SW	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay	City		250	250				500
		Grant		1,250	1,250				2,500
		Other							0
		Total	0	1,500	1,500	0	0	0	3,000
2.88 111th St. SW/ Lakeview Ave. SW - Kendrick to Perimeter	curb, gutter, sidewalks, sharrows ,street lighting, drainage, and patching	City							0
		Grant	2,000						2,000
		Other							0
		Total	2,000	0	0	0	0	0	2,000
TOTALS	City	1,060	1,174	1,912	659	850	1,110	6,715	
	Grant	10,824	6,403	9,031	3,010	3,300	3,840	36,308	
	Other	1,340	665	3,820	5,906	4,200	1,200	15,681	
	Total	13,224	8,242	14,763	9,575	8,350	6,150	58,704	

PROJECT COSTS IN THOUSANDS OF DOLLARS										
EXPENDITURE PLAN									TOTAL FUNDS	
SECTION 3 TRAFFIC SIGNALS			2017	2018	2019	2020	2021	2022	2017-2022	
3.1 Steilacoom / Durango Traffic Signal	Intersection meets warrants for traffic signal. Signal needed with new development in area. Special concern with adjacent train crossing becoming active.	City							0	
		Grant							0	
		Other	5	345						350
		Total	5	345	0	0	0	0	0	350
3.7 Washington Blvd. and Interlaaken Drive Signal and intersection improvement Total Estimated Cost \$375	Install new signal at intersection.	City			75	300			375	
		Grant							0	
		Other								0
		Total	0	0	75	300	0	0	0	375
3.8 Traffic Signal Timing Upgrades on-going technical support incl. turning movement counts	Upgrade traffic signal timing and coordination.	City	10	10	10	10	10	10	60	
		Grant								0
		Other								0
		Total	10	10	10	10	10	10	10	60
3.11 City-Wide Traffic Signal Management System Total Estimated Cost \$2,000	City-hall based Traffic Management Center. Fiber optic interconnect. PTZ major corridors. Active traffic management including web based info.	City	50	50	50	50			200	
		Grant		300	300					600
		Other								0
		Total	50	350	350	50	0	0	0	800
3.12 Traffic Signal Replacement Program	Replace aging traffic signals. Priorities based on maintenance history. (one signal every 3rd year)	City	300		300		300		600	
		Grant								
		Other								
		Total	0	0	300	0	300	0	0	600
3.13 Gravelly Lake Drive / Avondale Traffic Signal	Intersection meets warrants for traffic signal. Increased volumes in and around Towne Center. Increase in accidents.	City					100		100	
		Grant							0	
		Other						150		150
		Total	0	0	0	0	0	250	0	250

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 3			2017	2018	2019	2020	2021	2022	2017-2022
TRAFFIC SIGNALS									
3.14 So. Tacoma Way / 92nd Street	New warranted signal	City							0
		Grant		100	550				0
		Other							0
		Total	0	100	550	0	0	0	0
3.16 Steilacoom Blvd / Western State Hospital Signal Replacement	Replace existing signal	City							0
		Grant	210						210
		Other							0
		Total	210	0	0	0	0	0	210
3.17 Steilacoom Blvd / Lakeview Ave Signal Replacement	Replace existing signal	City							0
		Grant	275						275
		Other							0
		Total	275	0	0	0	0	0	275
3.19 Traffic Signal Asset Management System	Purchase software; develop asset management system	City	40	40	20	5	5	5	115
		Grant							0
		Other							0
		Total	40	40	20	5	5	5	115
3.20 Military Rd. and 112th St. Safety Improvement	Replace existing traffic signal to current standards. Update phasing to yellow-flashing arrow operation. ADA ramp upgrades. Repave intersection	City	15						15
		Grant	128	640					768
		Other							0
		Total	143	640	0	0	0	0	783
3.21 84th St. Pedestrian Crossing Signal at Pine St. S.	Install pedestrian signal, connection to Pine stree intersects Tacoma's Water Ditch Trail.	City							0
		Grant						110	110
		Other							0
		Total	0	0	0	0	0	110	110
TOTALS		City	415	100	455	365	415	125	1,575
		Grant	338	1,040	850	0	0	0	1,578
		Other	0	0	0	0	0	0	0
		Total	753	1,140	1,305	365	415	125	3,153

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 4 TRANSPORTATION PLANNING			2017	2018	2019	2020	2021	2022	2017-2022
4.1 Pavement Management System	Semi-Annual evaluation of pavement condition	City	5	30	5	30	5	30	105
		Grant							0
		Other							0
		Total	5	30	5	30	5	30	105
4.2 Transportation Model	On-going updates of travel demand model.	City	5	5	5	5	5	5	30
		Grant							0
		Other							0
		Total	5	5	5	5	5	5	30
4.8 Lakewood City Center Sub-Area Plan	Review access and circulation for vehicles, transit, and non-motorized transportation.	City	10	10					20
		Grant							0
		Other							0
		Total	10	10	0	0	0	0	20
4.9 Non-Motorized Transportation Plan Update	Update NMTP to include relevant policy updates and capital improvement projects. (original plan adopted June 2009)	City	10	10					20
		Grant							0
		Other							0
		Total	10	10	0	0	0	0	20
4.10 ADA Transition Plan Update	Update ADA transition plan to address ADA deficiencies of existing curb ramps; signal access / operations; etc.	City	15						15
		Grant							0
		Other							0
		Total	15	0	0	0	0	0	15
TOTALS		City	45	55	10	35	10	35	190
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	45	55	10	35	10	35	190

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 5 BIKEWAYS			2017	2018	2019	2020	2021	2022	2017-2022
5.1 Miscellaneous Bikeway Markings / Signage	City		20	20	20	20	20	20	120
	Grant								0
	Other								0
	Total		20	20	20	20	20	20	120
5.4 Miscellaneous Bike Lane Construction	City			50		50		50	150
	Grant								0
	Other								0
	Total		0	50	0	50	0	50	150
5.5 North Thorne Lane to Gravelly Lake Drive Non-Motorized Trail		Provide non-motorized path between Tillicum and Gravelly Lake Drive "Gravelly to Thorne Connector" construction.	City		20	30	350		400
			Grant		100	170	1,650		1,920
			Other			180	2,500		2,680
			Total	0	120	380	4,500	0	0
5.6A Gravelly Lake Non-Motorized Trail - Phase 1 (Washington Blvd to Nyanza (N)). <i>Amendment 1: Project split into three phases. Grant received for 5.6A.</i>		Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City	25	25	450			500
			Grant	125	125	2,390			2,640
			Other						0
			Total	150	150	2,840	0	0	0
5.6B Gravelly Lake Non-Motorized Trail - Phase 2 (Nyanza Blvd) <i>Notes: Project split into three phases. Grant received for 5.6A.</i>		Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City						0
			Grant						0
			Other			300	900	2,000	3,200
			Total	0	0	300	900	2,000	0
5.6C Gravelly Lake Non-Motorized Trail - Phase 3 (GLD - Nyanza (S) to Wash.) Construction 2022+. TOTAL Cost \$3.2 Mill. <i>Notes: Project split into three phases. Grant received for 5.6A.</i>		Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City						0
			Grant						0
			Other				300	900	1,200
			Total	0	0	0	0	300	900

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 5 BIKEWAYS			2017	2018	2019	2020	2021	2022	2017-2022
5.7 Motor Avenue - Whitman to Gravelly Lake Dr.	Provide non-motorized path including lighting and landscaping.	City	20	80					100
		Grant	180	650					830
		Other							0
		Total	200	730	0	0	0	0	930
		City	65	195	500	420	20	70	1,270
		Grant	305	875	2,560	1,650	0	0	5,390
		Other	0	0	480	3,400	2,300	900	7,080
		Total	370	1,070	3,540	5,470	2,320	970	13,740

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 6 STREET LIGHTING			2017	2018	2019	2020	2021	2022	2017-2022
6.2 New Street Lighting	Install street lighting in requested areas based on ranking criteria.	City	150	150	150	150	150	150	900
		Grant							0
		Other							0
		Total	150	150	150	150	150	150	900
6.6 LED Street Lighting Upgrades	Update existing PSE lighting.	City		250	250				500
		Grant							0
		Other							0
		Total	0	250	250	0	0	0	500
TOTALS		City	150	400	400	150	150	150	1,400
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	150	400	400	150	150	150	1,400

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 7 BRIDGES			2017	2018	2019	2020	2021	2022	2017-2022
7.1 Bridge Inspection	On-going biennial bridge inspection.	City	9	0	9	0	9	0	27
		Grant							0
		Other							0
		Total	9	0	9	0	9	0	27
TOTALS		City	9	0	9	0	9	0	27
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	9	0	9	0	9	0	27

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 8 BEAUTIFICATION PROJECTS			2017	2018	2019	2020	2021	2022	2017-2022
8.10 Gateway Improvements		City	50	50	50	50	50	50	300
		Grant							0
		Other	20	20	20	20	20	20	120
		Total	70	70	70	70	70	70	420
									0
									0
									0
									0
TOTALS		City	50	50	50	50	50	50	300
		Grant	0	0	0	0	0	0	0
		Other	20	20	20	20	20	20	120
		Total	70	70	70	70	70	70	420

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		<i>NOTE: BOLD and ITALICIZED numbers denote grant is secured</i>							TOTAL FUNDS
SECTION 9 ROADWAY RESTORATION PROJECTS			2017	2018	2019	2020	2021	2022	2017-2022
9.7 Resurfacing Program - Various Locations	Projects in various locations may include pavement preservation contribution to planned utility projects to facilitate full roadway overlays.	City	1,300	1,410	1,700	2,400	3,500	3,500	13,810
		Grant							0
		Other							0
		Total	1,300	1,410	1,700	2,400	3,500	3,500	13,810
9.10A Steilacoom Boulevard - 87th to Weller Road		City			20	350			370
		Grant				750			750
		Other							0
		Total	0	0	20	1,100	0	0	1,120
9.10B Steilacoom Boulevard - Weller Road to Custer Road Note: Overlay from 88th St to Custer included in project 2.29		City	20	350					350
		Grant		750					750
		Other							0
		Total	20	1,100	0	0	0	0	1,100
9.14 Lakewood Drive - 100th to Steilacoom Blvd		City	900						900
		Grant							0
		Other							0
		Total	900	0	0	0	0	0	900
9.15 Lakewood Drive - Flett Creek to N. City Limits		City		1,100					1,100
		Grant							0
		Other							0
		Total	0	1,100	0	0	0	0	1,100
9.16 59th Ave - Main Street to 100th Street		City			450				450
		Grant							0
		Other							0
		Total	0	0	450	0	0	0	450
9.17 108th - Bridgeport Way to Pacific Hwy		City			600				600
		Grant							0
		Other							0
		Total	0	0	600	0	0	0	600

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN		<i>NOTE: BOLD and ITALICIZED numbers denote grant is secured</i>						TOTAL FUNDS	
SECTION 9 ROADWAY RESTORATION PROJECTS			2017	2018	2019	2020	2021	2022	2017-2022
9.18 Custer - Steilacoom to John Dower	City					450			450
	Grant								0
	Other								0
	Total		0		0	450	0	0	450
9.19 88th - Steilacoom to Custer	City			250					250
	Grant								0
	Other								0
	Total		0	250	0	0	0	0	250
9.20 Pacific Hwy - 108th to SR512	City				90				90
	Grant				450				450
	Other								0
	Total		0		540	0	0	0	540
9.21 100th - Lakeview to South Tacoma Way	City				180				180
	Grant				300				300
	Other								0
	Total		0		480	0	0	0	480
9.22 100th - 59th to Lakeview	City						1,100		1,100
	Grant								0
	Other								0
	Total		0		0	0	1,100	0	1,100
TOTALS	City		2,220	3,110	3,040	3,200	4,600	3,500	19,650
	Grant		0	750	750	750	0	0	2,250
	Other		0	0	0	0	0	0	0
	Total		2,220	3,860	3,790	3,950	4,600	3,500	21,900

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 11 OTHER			2017	2018	2019	2020	2021	2022	2017-2022
11.1 On-call technical assistance	Various professional services including surveying, structural, geotechnical, environmental to support various projects.	City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total		50	50	50	50	50	50
11.2 Public Works Operations & Maintenance Facility	Back up generator and fueling station.	City			200				200
		Grant							0
		Other							0
		Total		0	0	200	0	0	0
TOTALS		City		50	250	50	50	50	450
		Grant							0
		Other							0
		Total	0	50	250	50	50	50	450

ARTERIAL STREETS

	2017	2018	2019	2020	2021	2022	2017-2022
City	152	402	102	202	927	352	2,137
Grant	500	1,300	0	300	1,475	2,650	6,225
Other	50	2,500	13,500	13,500	575	250	30,375
Total	702	4,202	13,602	14,002	2,977	3,252	38,737

ROADWAY IMPROVEMENTS

	2017	2018	2019	2020	2021	2022	2017-2022
City	1,060	1,174	1,912	659	850	1,110	6,715
Grant	10,824	6,403	9,031	3,010	3,300	3,840	36,308
Other	1,340	665	3,820	5,906	4,200	1,200	15,681
Total	13,224	8,242	14,763	9,575	8,350	6,150	58,704

TRAFFIC SIGNALS

	2017	2018	2019	2020	2021	2022	2017-2022
City	415	100	455	365	415	125	1,575
Grant	338	1,040	850	0	0	0	1,578
Other	0	0	0	0	0	0	0
Total	753	1,140	1,305	365	415	125	3,153

TRANSPORTATION PLANNING

	2017	2018	2019	2020	2021	2022	2017-2022
City	45	55	10	35	10	35	190
Grant	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0
Total	45	55	10	35	10	35	190

STREETLIGHTS

	2017	2018	2019	2020	2021	2022	2017-2022
City	150	400	400	150	150	150	1,400
Grant	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0
Total	150	400	400	150	150	150	1,400

BRIDGES

	2017	2018	2019	2020	2021	2022	2017-2022
City	9	0	9	0	9	0	27
Grant	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0
Total	9	0	9	0	9	0	27

BEAUTIFICATION / GATEWAY IMPROVEMENTS

	2017	2018	2019	2020	2021	2022	2017-2022
City	50	50	50	50	50	50	300
Grant	0	0	0	0	0	0	0
Other	120	20	20	20	20	20	120
Total	70	70	70	70	70	70	420

RESTORATION

	2017	2018	2019	2020	2021	2022	2017-2022
City	2,220	3,110	3,040	3,200	4,600	3,500	19,650
Grant	0	750	750	750	0	0	2,250
Other	0	0	0	0	0	0	0
Total	2,220	3,860	3,790	3,950	4,600	3,500	21,900

BIKEWAYS

	2017	2018	2019	2020	2021	2022	2017-2022
City	65	195	500	420	20	70	1,270
Grant	305	875	2,560	1,650	0	0	5,390
Other	0	0	480	3,400	2,300	900	7,080
Total	370	1,070	3,540	5,470	2,320	970	13,740

OTHER

	2017	2018	2019	2020	2021	2022	2017-2022
City	0	50	250	50	50	50	450
Grant	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0
Total	0	50	250	50	50	50	450

NEIGHBORHOOD TRAFFIC MANAGEMENT

	2017	2018	2019	2020	2021	2022	2017-2022
City	25	25	25	25	25	25	150
Grant	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0
Total	25	25	25	25	25	25	150

GRAND TOTAL (2016-2021)

	2017	2018	2019	2020	2021	2022	2017-2022
City	4,191	5,561	6,753	5,156	7,106	5,467	33,864
Grant	11,967	10,368	13,191	5,710	4,775	6,490	51,751
Other	1,510	3,185	17,820	22,826	7,095	2,370	53,256
Total	17,568	19,114	37,764	33,692	18,976	14,327	138,871

Notes:

LID = Property owner participation through a Local Improvement District (LID).
Dev. Contr. = Funds provided through private (developer) contribution
TIB = Transportation Improvement Board grant funding
TEA-21 = Transportation Efficiency Act grant funds.
State = other state grant funding programs
CDBG = Community Development Block Grant funds.
FED = Federal Grant dollars (TEA-21, SAFETEA, Enhancement, etc.)
SWM = Surface Water Management funds
S.T. = Sound Transit
TBD = Transportation Benefit District
MAP-21 = Moving Ahead for Progress in the 21st Century (Federal Transportation Act)